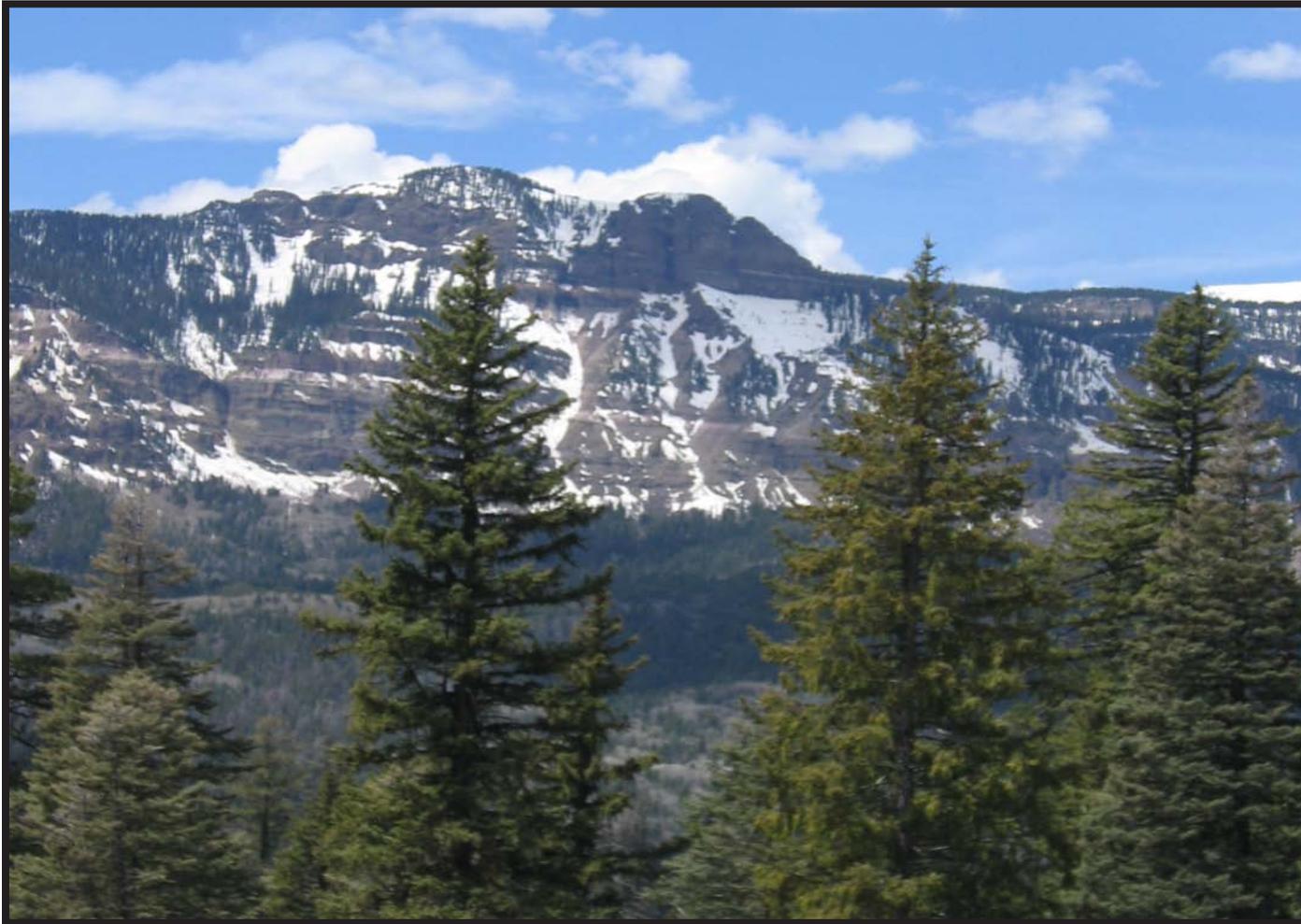


# TOWN OF RANGELY TRAILS MASTER PLAN



Trails Master Plan  
Rangely, Colorado  
Prepared by DHM Design Corporation  
August 2011: DRAFT FOR REVIEW



*"The influence of fine scenery, the presence of mountains, appeases our irritations and elevates our friendships."*

*--Ralph Waldo Emerson, The Conduct of Life, 1860*



The Town of Rangely, in Colorado's Rio Blanco County, lies along the scenic Dinosaur Diamond Scenic and Historic Byway and can truly be considered the front door to Dinosaur National Monument. Outdoor recreation opportunities shape the quality of life in Rio Blanco County and Rangely in particular. Although small in population, the residents and community leaders in Rangely have always strived to develop high quality amenities for residents and visitors.

For a number of years, engaged, community-minded folks here visualized and planned for a better future—one that builds on unique natural assets and outdoor values. Equally important is the realization that, economically, the Rangely area needs to invest in infrastructure that helps it remain competitive with other mountain and resort communities in order to thrive—especially in light of the current global real estate market and economic challenges.

A vital part of this vision is to create a high-quality multi-use trail system that offers a way for both residents and visitors to enjoy the area at a pace that matches a special local way of life. Currently, getting around the Rangely area without a motor vehicle can be daunting, hazardous, and unpleasant. Over the years a number of plans have been put forth and some were realized, at least in a modest first step.

The culmination is this master plan provides a specific road map for taking the Town recreation trails vision to reality. The goal is to create and realize a safe, reliable, pleasant and affordable multi-use trail system that can be used by everyone, from children to avid outdoor recreationalists, connecting the community and highlighting the area's innate natural beauty. It envisions an amenity that can be built over the next several years with great benefit to all, today and for many years to come.







#### Key Officials, staff and contributors

The participants in the Key Stakeholder meetings, public meetings and meetings with individuals included neighbors; corridor property owners and businesses; recreationalists and numerous others who shared their advice and expertise.

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*This plan was funded by the Town of Rangely and a grant from The Great Outdoors Colorado State Trails Program.*

*Special thanks to the Key Stakeholder Committee for valuable input that help shape the final alignment of the trails throughout and around the Town of Rangely.*

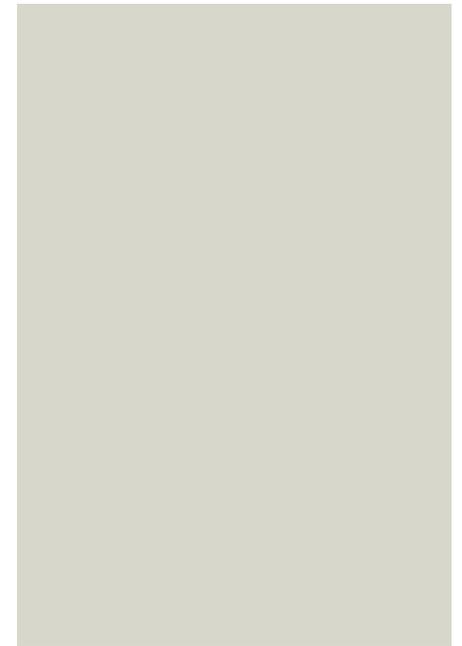




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# CHAPTER ONE: INTRODUCTION



Mission Statement  
Purpose of this Document  
and Background  
Study Area and Corridor Description  
The Planning Process and  
Community Engagement



### Mission Statement

Create a safe, affordable, practical and continuous, multi-use (10'-wide) trail alignment that connects various key destination points within the Town. The aim is to provide and promote enhanced recreational and fitness opportunities, access to safer and cleaner forms of transportation, increased community connectivity, business development and community identity.

### Purpose of this Document and Background

This document puts forth a master plan for the Town of Rangely that includes specific feasible alignments for the trail system, trail cross section concepts, cost estimates and phasing priorities suitable for raising funds and to support future preparation of construction plans. It is intended to guide, step-by-step, the creation of a trail system throughout and around the Town and connecting to recreational amenities in the surrounding County and on BLM lands. The proposed route hopes to have anchors at pertinent locations such as the Colorado Northwestern Community College, Rangely Museum, Camper Park, the White River, Western Rio Blanco Recreation Center, and both public schools. It also strives to access scenic and more rural areas within the County and on BLM lands.

With its ability to provide community access to vital locales, the Town of Rangely trails will be a very substantial investment and asset, serving residents of the Rangely area, visitors and tourists and the wider Rio Blanco County region. This project offers unique potential on a number of levels.

- It will provide a safe, reliable, outdoor, non-motorized avenue for users to travel throughout the town of Rangely as well as accessing adjacent recreation areas, schools, and other amenities.
- It will provide trail access to scenic areas rich in history and interpretive value.
- It will help promote businesses, such as restaurants and shopping along the Main Street corridor, as well as enhance community development.
- It will provide access to the Colorado Northwestern Community College, High School / Middle School and Elementary School, Elks Park, the Recreation Center and other destinations.
- It will minimize and mitigate automobile conflicts.

- It will increase property values for adjoining properties, promote business development and enhancement, and assist Rangely in attracting visitors as a Colorado destination.
- It will promote better community health through fitness and regular physical activity.
- It can potentially save on fuel costs—both to individuals and institutions such as the School District by offering an alternative mode of travel.
- This plan also proposes to promote—through increased public awareness and cooperative planning with adjacent landowners—the stewardship, clean-up and enhancement of the White River corridor environment, especially the scenic character, as well as the flora, fauna and habitat values.
- It will define improvements to the existing trail along Kennedy Drive in order to encourage the use of this trail to minimize pedestrian and vehicle conflicts.
- It will define a new loop trail within Elks Park, a key amenity within the Town.

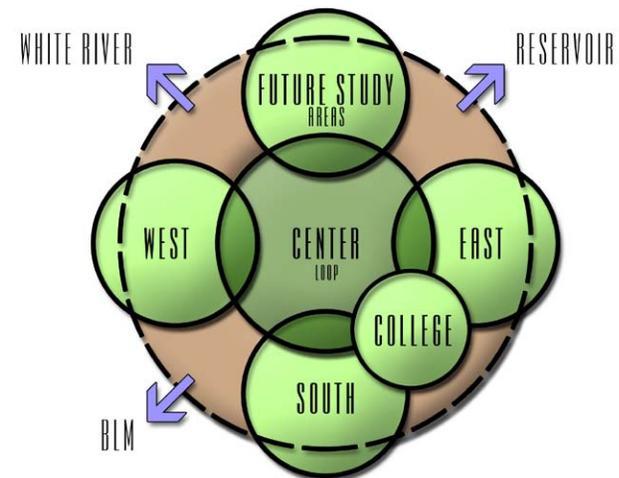


### Study Area, Corridor Descriptions and Planning Elements

The study area is primarily within the Town limits and immediately adjacent to the Town. The primary goal of the plan is to connect several key places within the Town as well as provide access to key open space areas surrounding the Town, therefore the study area is focused on trail “loops” within and adjacent to the Town. Within the master plan study, additional detail has been provided for two areas within the Town that are considered first priority trails; improvements to the Kennedy Drive Trail and a loop trail within Elks Park.

The master plan also includes a trail loop to the south of the Town through BLM lands. This “South Loop” connects the Town to a frequently used section of BLM lands with a unique character compared to most portions of the trail system. In addition, trail connections to outlying trails on BLM lands are indicated, expanding the breadth of the possible trail routes significantly.

Lastly, the County lands directly north of the town limits are identified as a future study area. This area is of particular interest as it contains the White River corridor and all its associated recreational opportunities. This area will require significant clean up prior to allowing public access, but may someday be a valuable addition to the trails system.



Conceptual Diagram



The planning area was divided into segments or “loops” defined by logical connections of local destinations and the special character and distinguishing features of each loop. Ideally, each loop of the trail can be a complete, logical section in itself. The finished project will be a step-by-step process with each step being complete and useful in its own right. The loops are:

- West Loop: West of Stanolind Avenue
- Center Loop: Stanolind Avenue to Camper Park
- College Loop: The end of Ridge Ave to Kennedy Drive
- East Loop: Kennedy Drive to Highway 64
- South Loop: Connects all other loops to a trail through BLM lands
- Future Study Area: A future North Loop along the White River
- Outlying Connections: North BLM lands and the trail to Kenney Reservoir



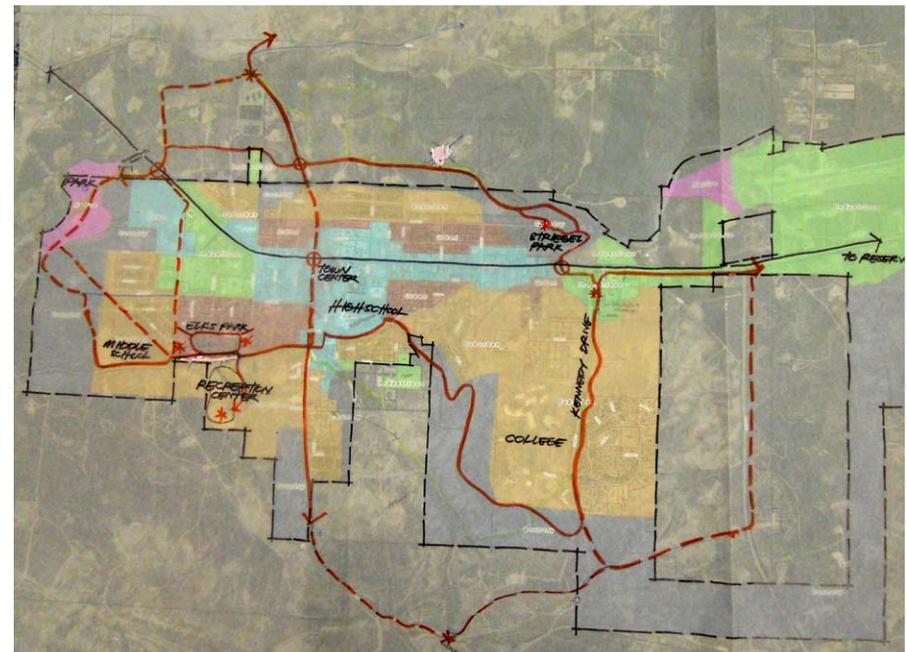
## The Planning Process and Community Engagement

The planning process included four major elements:

1. **Site Inventory and Reconnaissance** - A thorough inspection and inventory of the resources, challenges, and opportunities of the corridors. Site investigation included a number of field visits where the planning team inspected the possible trail locations. The planning team identified opportunities and constraints and reviewed potentials with community leaders, key staff and the public.
2. **Meetings with Peter Brixius and Vicky Pfennig** - Review Plans, tour potential trail alignments and determine aspirations for the trails Master Plan.
3. **Draft Alignment Alternatives, Layout and Cross-Sections** - Working with Town staff and other stakeholders, the team laid out draft optimal trail alignments and prepared typical cross-sections.
4. **Public and Stakeholder Participation Process** - A public participation process that consisted of two key stakeholder meetings and two public meetings with the Town Council. Attendees consisted of individuals, property owners, user groups, school district representatives and business owners who were invited to candidly review and discuss the trail vision. After the public review sessions a final draft plan was prepared and reviewed with Town officials.

Meetings were well attended with 20 participants in the first Key Stakeholder meeting and 12 in the second. Nearly all of the participants were engaged and enthusiastically supportive, though some expressed concerns and many suggestions were received. Changes were made to the plan to accommodate the many good ideas as well as the concerns heard at the meetings.

5. **Final Master Plan Submittal** - The Planning team submitted this report summarizing the Town of Rangely Trail Master Plan with estimates of probable costs and phasing strategies.

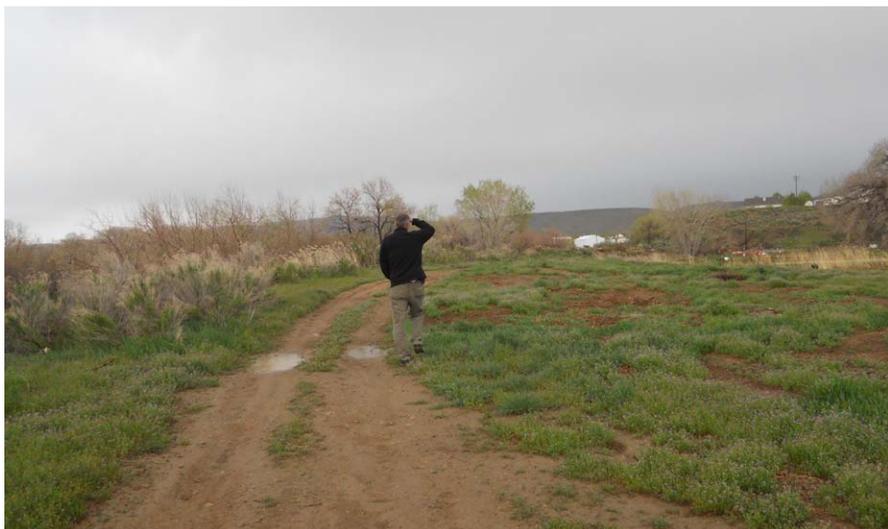


### Needs Assessment

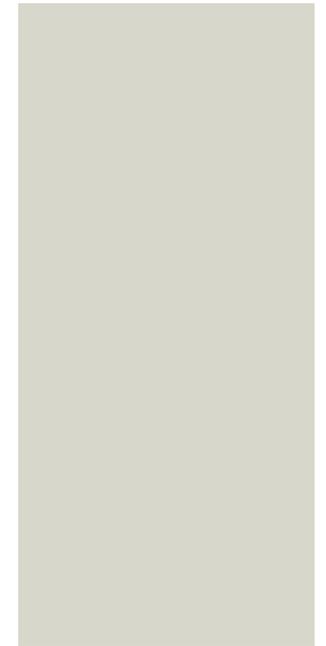
There are several ways to identify needs for trails in a community and specifically the need for Rangely's network of trails. These include: citizen surveys, input through public participation processes such as community meetings and Web sites, and comparison to published standards for level of services (i.e. recommended miles of trails per 1000 population).

While the planning process, due to budget constraints, did not include a scientific community survey, conclusions about trail needs can be reached considering a number of sources. Foremost, people who attended the community public workshops demonstrated substantial support.

While the state-of-the-art in measuring community needs, standards for multi-use trails is still somewhat primitive, there are some published recommendations. For example, the State of Colorado Small Community Park and Recreation Planning Standards (2003) recommends approximately 1 mile of paved multi-use trails per 1000 population. Based on this standard given a population of over 2,000, Rangely is in need of a designated multi-use trail system.



# CHAPTER TWO: GUIDING PRINCIPLES AND COMPONENTS

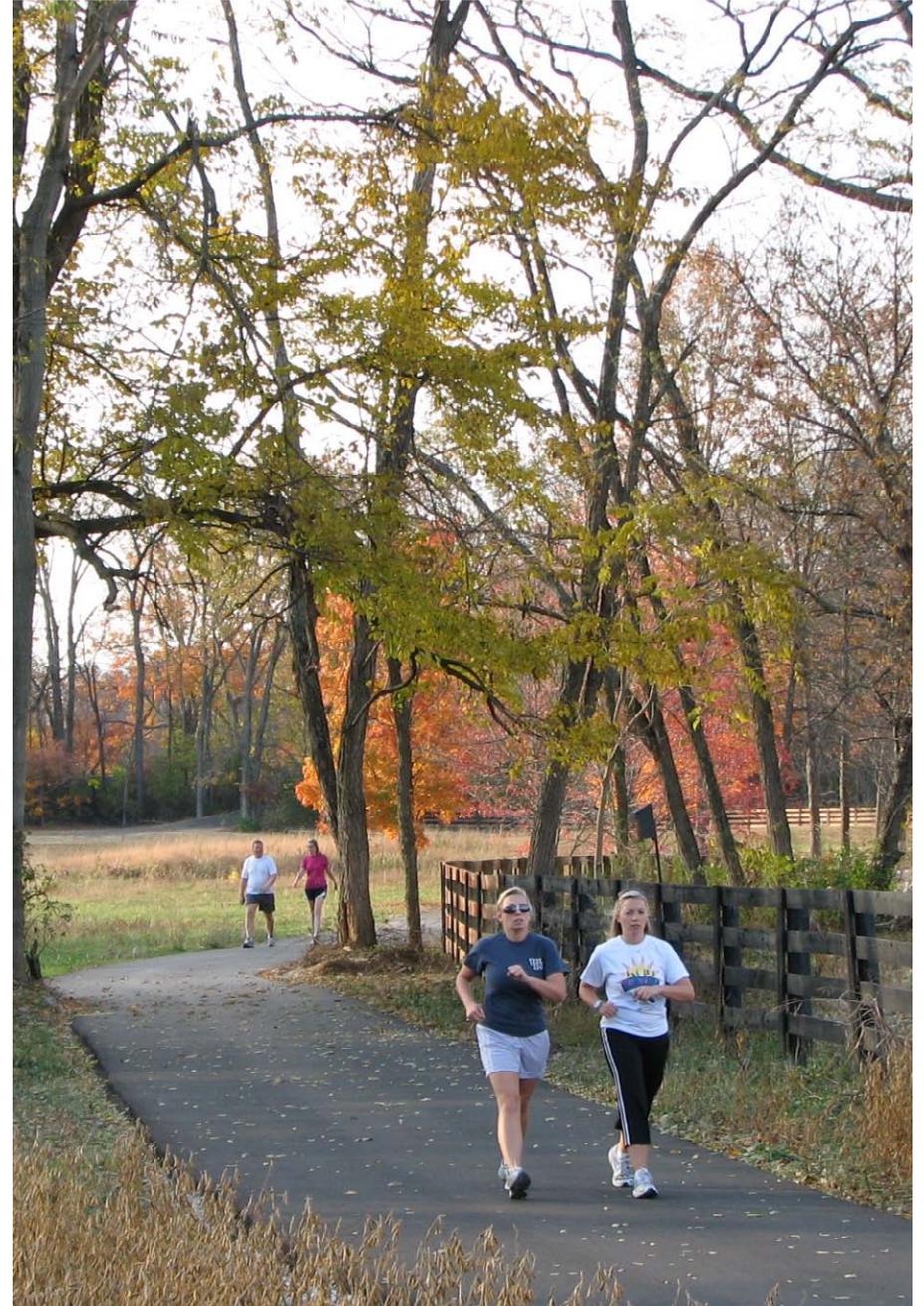


Guiding Principles  
Trail Components

With its unique character, central location and proximity to both urban and open space districts, the Town of Rangely trails will be a substantial asset for present and future generations. The trail loops will provide a reliable, efficient, scenic, non-motorized alternative for traveling in and around Rangely.

To fully benefit from the opportunities afforded by these trails (the trail itself, connections to civic destinations, and access to scenic areas) the project must result in a net improvement to the setting. That is, the trail improvements should enhance the Town and character of the neighborhoods, offer an outstanding user experience, and continue to promote the beneficial use and enjoyment of adjacent properties. The ethic of leaving it better than we found it should prevail throughout the process.

The trails must be safe (within the parameters of state-of-the-art design standards), functional, and convenient for trail users and nearby residents. It must also be affordable to build and maintain. The guiding principles, components, and design standards presented below are benchmarks used to achieve these goals. These principles grew out of consultation with citizens, property and business owners, and government agencies in concert with the technical expertise and experience of the planning and design team.

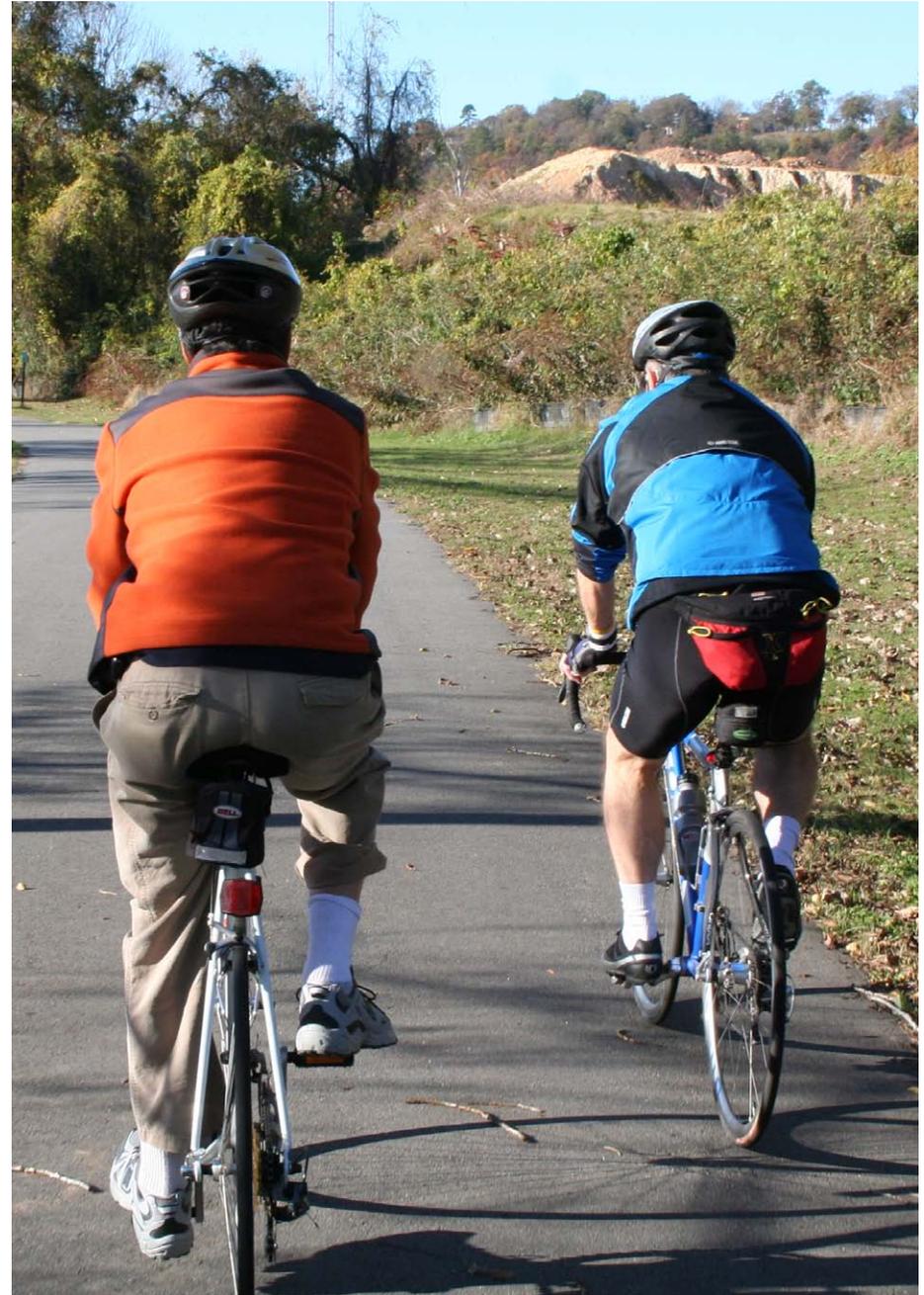




### Guiding Principles

1. The trail system should offer an outstanding experience including natural vistas and, wherever feasible, gentle grades and solace from motor vehicle noise and fumes. It should also offer an enjoyable practical option for non-motorized commuting.
2. Where applicable, provide a trail that promotes child-friendly transportation and safe access to schools.
3. The trail system should accommodate a full range of uses (and be readily accessible per the Americans with Disabilities Act) including but not limited to runners, bikers, walkers, skaters, skateboarders, wheelchairs and other non-motorized uses including a full range of abilities.
4. Promote trail connectivity to nearby neighborhoods, schools and to the regional trail network.
5. The project must be affordable to build and maintain and appeal to potential outside funding sources, both public and philanthropic.
6. Promote non-motorized community connectivity and fitness through physical activity.
7. Provide an attraction for tourism, helping to make the Rangely area more competitive as a recreational destination as well as fostering identity as a home for outdoor recreation enthusiasts.
8. The trail and conservation improvements should be feasible to accomplish in the near term with expansion in the future.

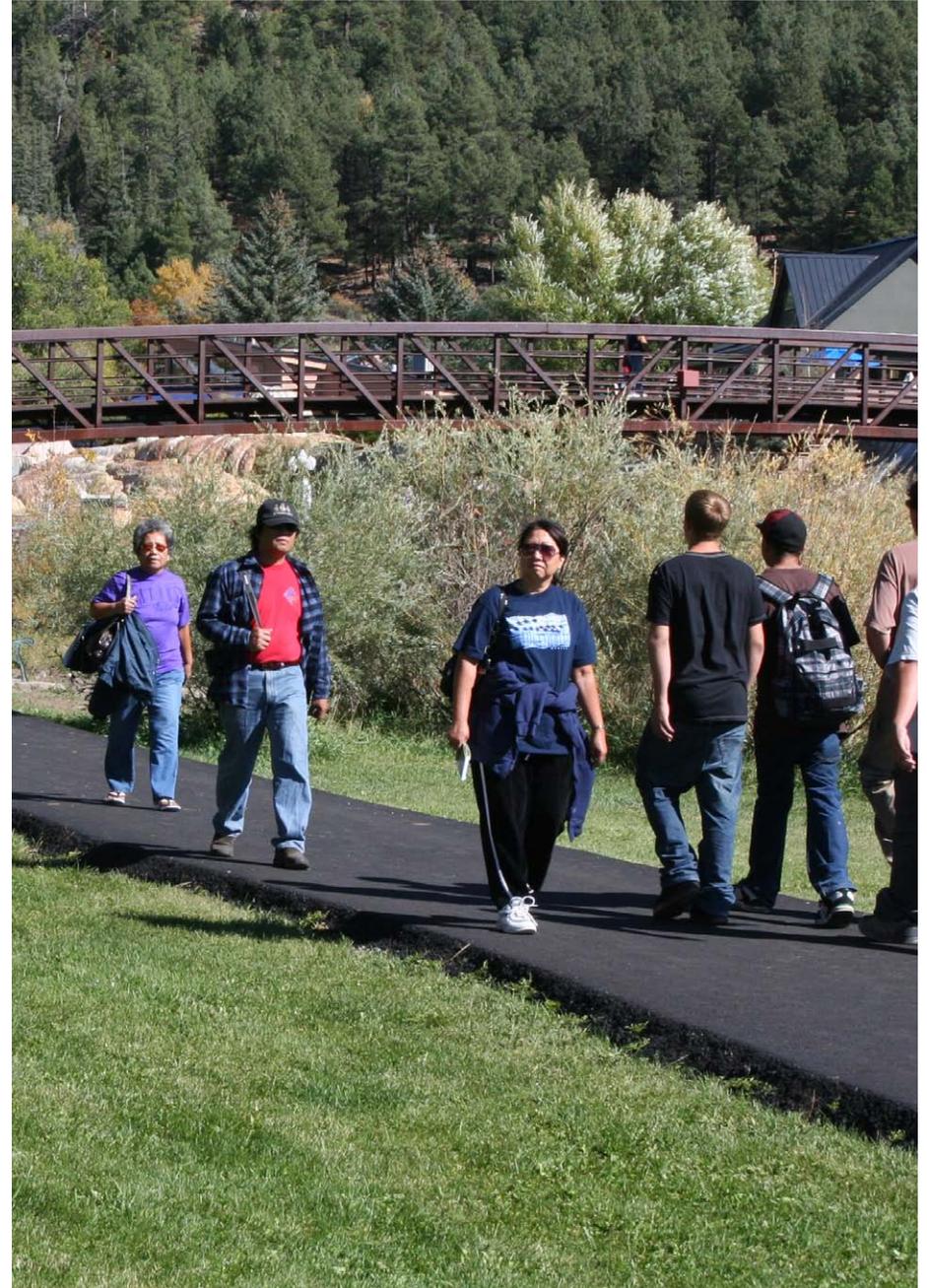
- 9. The trail must be designed and maintained to respect private property and to be a good neighbor!
- 10. All improvements should promote the preservation and enhancement of flora, fauna, habitat and cultural resources.
- 11. Offer opportunities for youth, student and volunteer stewardship activities.
- 12. Offer educational/interpretive opportunities (ecology, history, culture)



### Trail Types, Components and Cross Sections

This plan recommends several key elements or components which function as building blocks for the trail system. A list of components was generated considering field conditions, the wishes expressed at community meetings and other input. The descriptions, plans, and cross section drawings below specify, in general, the recommended elements. Note that these are for planning and budgeting purposes and not engineering drawings. Specific designs, specifications and detailing will occur during the design and construction phase\*.

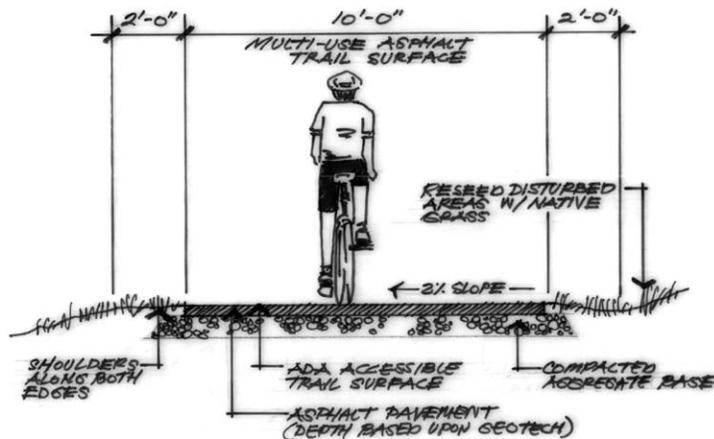
\* For more details, standards and design references see: *Guide for the Development of Bicycle Facilities*, American Association of State Highway and Transportation Officials (AASHTO); *Trails for The 21st Century*, Rails to Trails Conservancy and Island Press; and [www.americantrails.org](http://www.americantrails.org); See also *Manual of Uniform Traffic Control Devices (MUTCD)* U.S. FHWA, [mutcd.fhwa.dot.gov/](http://mutcd.fhwa.dot.gov/) (for signage and other traffic regulation-related features for both automobile and bicycle facilities), Note too that, as of late 2009, new guidelines for accessibility per the Americans with Disabilities Act were in the process of update see [www.access-board.gov](http://www.access-board.gov)



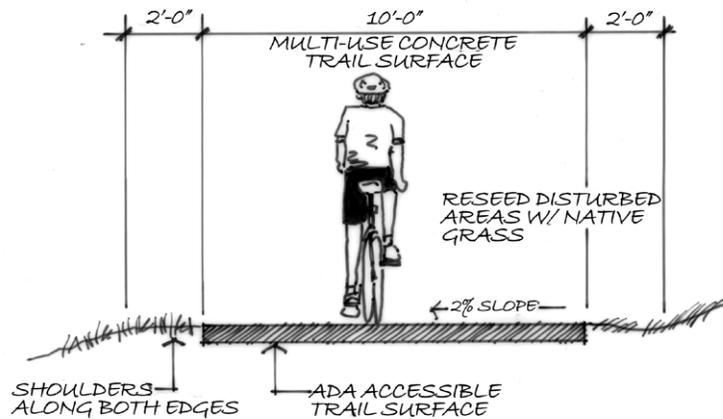
## TYPICAL ELEMENTS

## Paved Multi-Use (Shared-Use) Trail

There are two paved trail surface options—asphalt and concrete. Typically, for cost and other reasons, asphalt has been the surface of choice on many mountain community trails in Colorado. However, it should be noted that because of its durability and lower maintenance requirements, concrete has certain advantages for trail projects. Generally, concrete is recommended for areas subject to frequent inundation or erosion such as along a stream.

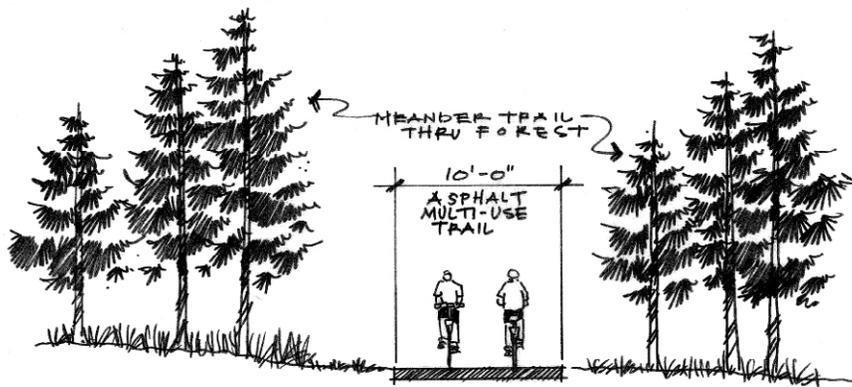


Typical Cross Section - Asphalt Multi-Use Trail



Typical Cross Section - Concrete Multi-Use Trail



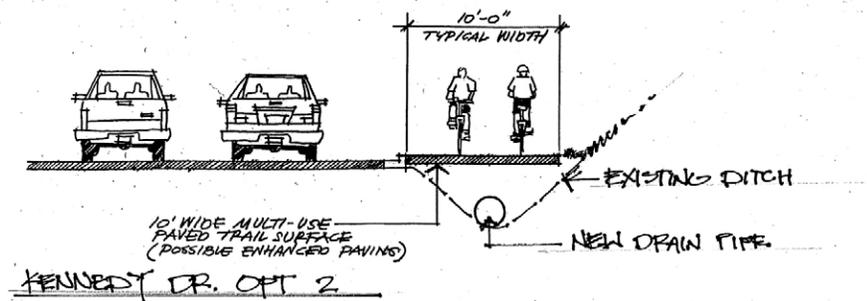
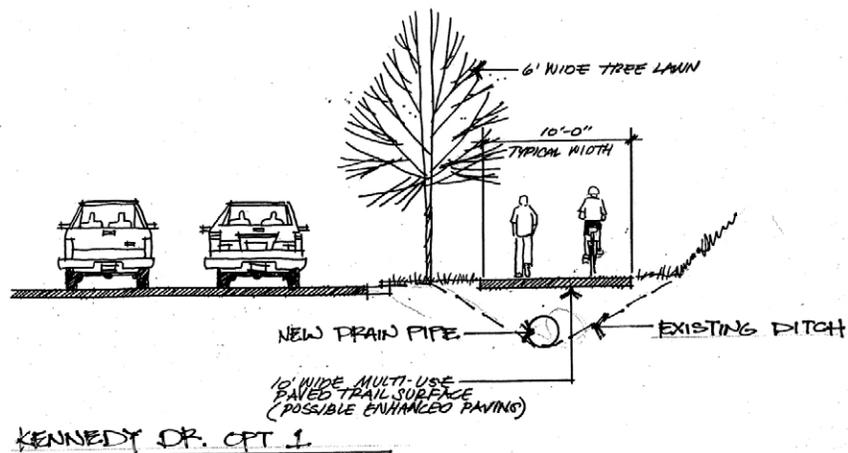


Paved trail surfaces accommodate pedestrian, bicycles, skates, and wheelchairs. Typically the paved surface is 10'-wide and designed to national engineering (AASHTO for Bicycles) and Americans with Disability Act accessibility standards. There is a graded trail edge on either side between 24" and 5'-wide with 5' preferred. This shoulder area should be mowed and kept free of debris though the width of the mowed area may undulate for improved aesthetics. Typically, grades do not exceed 5% with up to 10% for very short distances. For purposes of this plan, the shared-use path, when adjacent to a roadway includes a 5' to 10'-wide (minimum) landscaped buffer between the trail and the adjacent road. There should be a 30" minimum buffer between the trail edge and adjacent fences, walls or other obstructions.



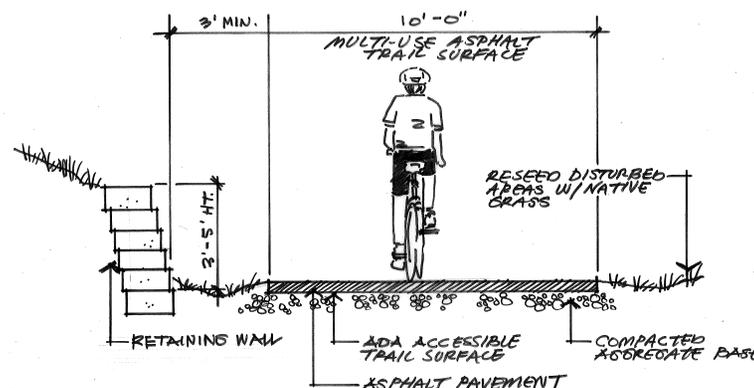
Roadside (Shared-Use) Trail

In a number of places, such as certain locations along Kennedy Drive and Main Street, the paved multi-use trail will run parallel and proximate to a roadway. Typically this consists of a 10'-wide "paved" surface with a vegetated or landscaped buffer (5' or more in width preferred) between the edge of the road and the trail. The trail allows for two-way bicycle and pedestrian traffic. If a 5'-wide buffer cannot be achieved than a minimum 42" high safety barrier or 6" vertical curb should be provided per AASHTO guidelines.



Paved Trail with Retaining Wall

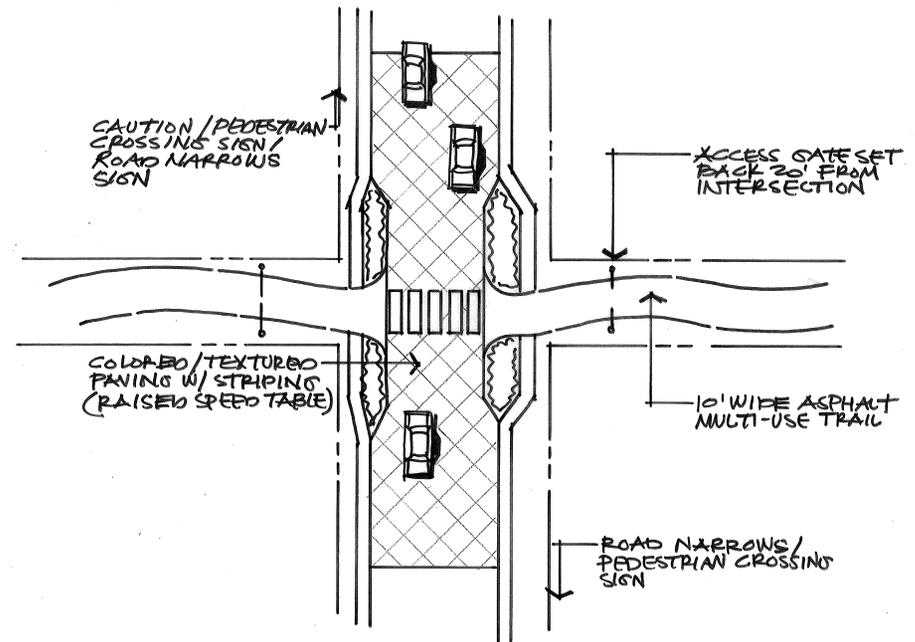
This is a paved trail with an integral or proximate retaining wall. This wall may be 3' to 5' high. Where the trail surface is asphalt, the wall is a separate structure. In some instances, where the trail surface is concrete, the wall is typically built as an integrated unit where it is "keyed" into the trail surface and/or connected with re-enforcing steel rods. The trail surface is typically 10'-wide and there may be a safety guardrail with a steep drop-off or where other hazards exist.



### At Grade Street Crossings and "Traffic Calming" Street/Trail Intersection

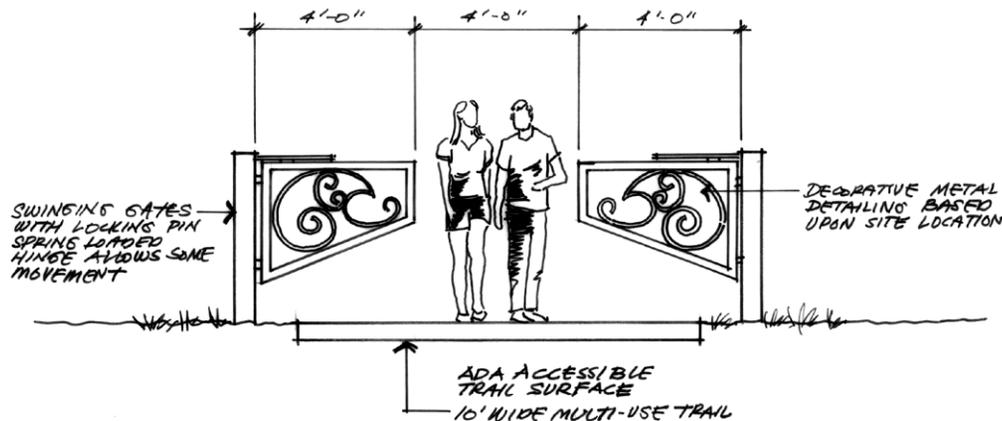
There will be several instances where the trail system will cross streets - perhaps at intersections controlled by either a traffic light or by stop signs. In the instances of busier roads, like Main Street (Hwy 64), crossings are ideally traffic-light controlled, with a pedestrian activated signal for trail users. In our discussions with CDOT, they have advised signage at trail crossings and warning signs prior to reaching the trail crossings. ADA accessible curb ramps should be provided and crosswalk striping at road crossings. Where traffic lights are not practical, and cross traffic does not stop, then intersections should be well signed to warn trail users to yield to traffic and use caution crossing.

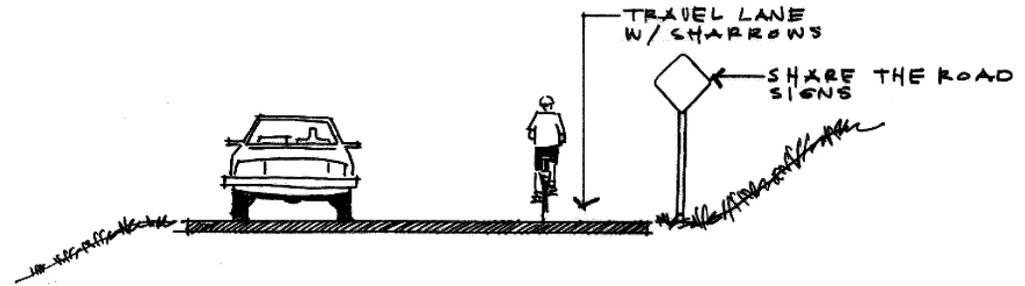
There might also be trail crossings along lower volume streets. In these instances, particularly mid-block, a layout that promotes a safe interaction for both bike and pedestrian trail users should be provided. This consists of warning signs and striped crossing markings on the pavement per the Manual of Uniform Traffic Control Devices MUTCD, a neck down that narrows the traffic lane, special texturing of both the street and the trail approach and/or possibly a raised pavement "speed bump" or "speed table" to alert and slow motorists.



### Access Gates

Access gates restrict automobile and most other motorized vehicles entry to trail corridors. The gate is designed with a lock and is hinged for easy entry by authorized personnel. Typically the gate is set back from the adjoining street with enough clearance to allow most vehicles to safely pull off the street to open the gate. The gate is substantial enough to discourage removal or damage. A gap is left in the gateway that allows a bicyclist or pedestrian to pass through, but not a motor vehicle. Safety and regulatory signage, in compliance with the Manual of Uniform Traffic Control Devices (MUTCD), used on the street, alerts motorists just as similar signage on the trail alerts trail users to the gateway. Signage is placed an adequate distance ahead to allow response time and is designed per the MUTCD. In some instances, fencing should be used beyond the gate to further define the restricted access to the trail.





#### On-Street Shared-Use Routes

In addition to the “backroads” network, there are a number of low volume streets such as Stanolind Avenue. Typically these are where bicyclists and pedestrians share a lower volume, lower speed (25 mph) street with automobiles. This may consist of “share-the-road” yellow diamond caution signs with a bicycle symbol and placards that “spell out” “share the road”. In some instances, there may be a designated bike lane defined by either a solid white paint strip or painted bicycle symbols applied to the pavement to designate bicycle use. Designs are per the US Manual of Uniform Traffic Control Devices (MUTCD) and the AASHTO Guide to the Development of Bicycle Facilities. Optimally, a shared lane is 14'-wide, not including the gutter pan if there is no parking lane and where there is not a designated bike lane.

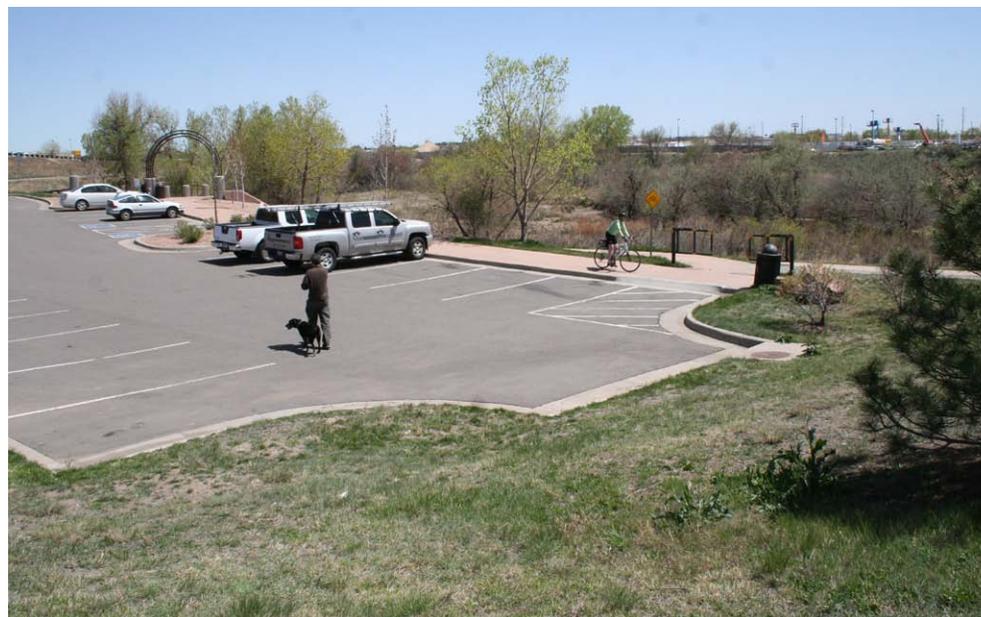
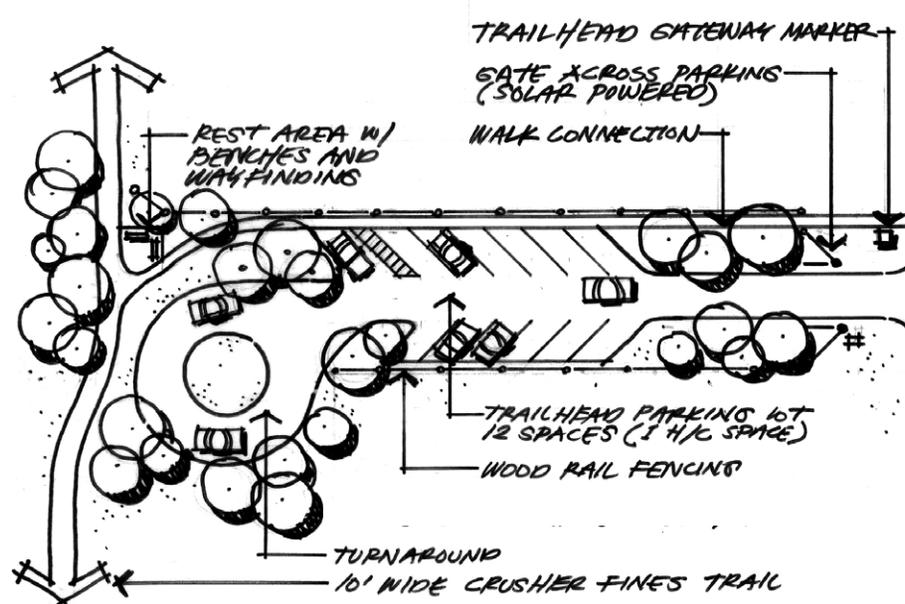
**Trailheads and Entry Features**

Trailheads should be strategically located where users might logically want to access the corridor by automobile and park to bike or hike. Typically these could accommodate 10 to 30 automobiles and could have a paved or gravel surface. Trailheads should also include an entry monument or sign that includes a trail system map, with "you are here" marker, and applicable user courtesy/regulations and other information. These locations could also include restrooms, shade structures and drinking water.

In some instances entry points might be more elaborately improved to enhance trail visibility to the public. These entry features might include special landscaping, trim elements, shade structures, and sculptural elements.

In other instances the trail entry point might not offer parking, serving rather as a "walk-up" or "bike-up" point of entry. These should include an accessible ramp from the street where applicable and neighborhood-appropriate signs or small pylons indicate the entry point. A small system map at these locations will also help with wayfinding.

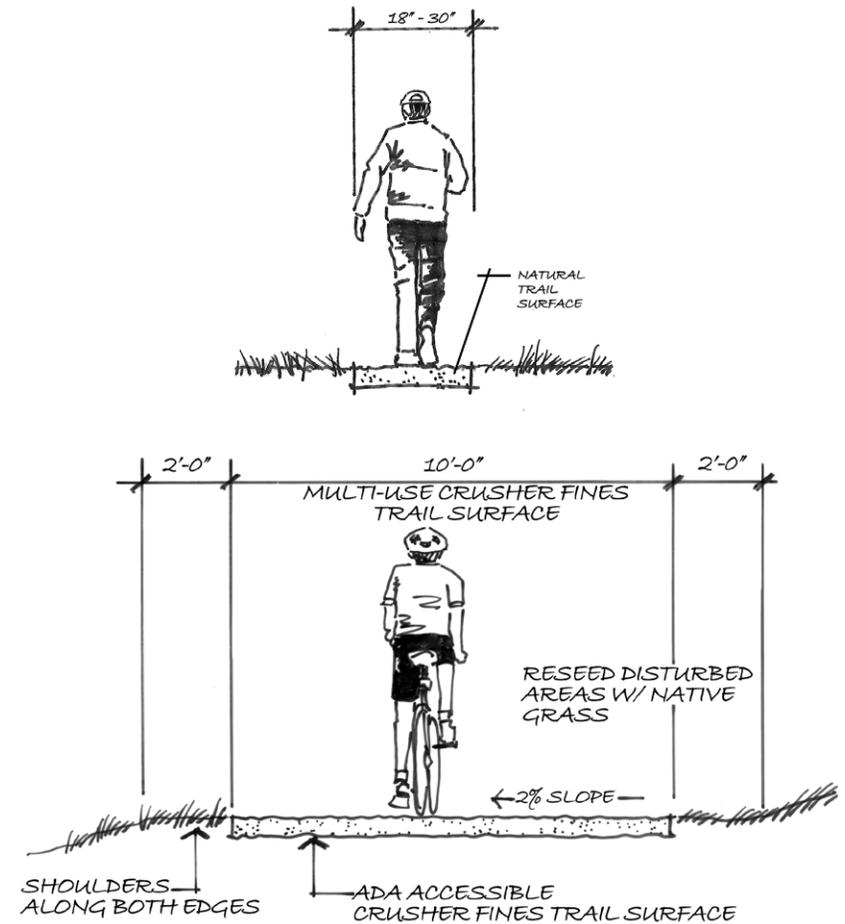
Typically, trailheads, entry features, and other points where people can park or congregate, should not be placed in close proximity to residences. A gate that closes the area at night can be provided. A number of communities have used solar-activated automatic gates that close at sunset preventing access after dark.





### Natural Surface Trail

This is a graded dirt surface with appropriate erosion control and stabilization. Width may vary from 18" to 10', depending on permitted use, this trail accommodates hikers, mountain bikes, all-terrain wheelchairs, and equestrians. It does not meet national (AASHTO) standards for bicycles. Typically grades do not exceed 5% with 12.5% the maximum for short distances.



**Signage; Safety, Courtesy, Interpretive and Wayfinding System**

A number of informational, educational, interpretive and way-finding devices are recommended for the trail corridors. These include:

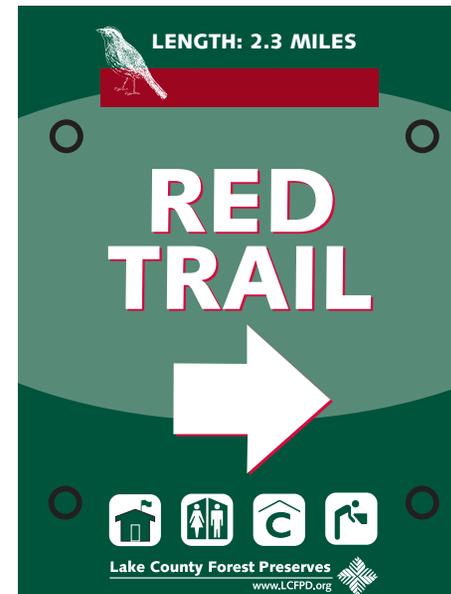
**Safety Signs**

These signs and/or pavement markings address or promote trail user and bicycle safety. For ease of understanding, these signs should follow standard formats for traffic control devices (See Manual of Uniform Traffic Control Devices). Signs address both bicycle and automobile traffic signage with respect to both trails and shared on-street routes.



**Wayfinding/Directional Signs**

Includes signs and markers, some with maps showing trail users how to reach their destinations, distance from a destination, and location signs such as mile markers, and street signs placed on bridges to identify cross streets. The wayfinding system should include overview signs and maps used at major entries (The system map should also be readily accessible on the Web). They address comprehensive issues such as system-wide trail maps, location of rest areas, degree of difficulty, accessibility and system trail rules and regulations. Due to the amount and importance of the information conveyed on system signs, it is best to place them in locations where users are encouraged to safely stop and review the information represented. Markers may have a specific logo, or they may be as simple as blaze, using pieces of brightly colored tape attached to sign posts to indicate the corridor (used extensively in France and other places).



**Credit Signs**

Provides information about those who contributed to the development of the trail and/or amenities along the trail.

**Interpretive signs and displays**

Addresses natural and/or cultural features. Important topics include ecological and geophysical interpretation and history.

### Trail Lighting

Due to its use well into the evening hours and distance from adequate street lighting, it is recommended that the trail along Kennedy Drive should be lighted. The lights should be lower in height than typical street lights, 14 to 16 feet in height is ideal and up to 20 feet tall at a maximum. Light levels should be as even as possible and even short unlit sections should be avoided. The lighting will create a safer and more enjoyable user experience with the primary goal of moving pedestrians and bike riders off Kennedy Drive.



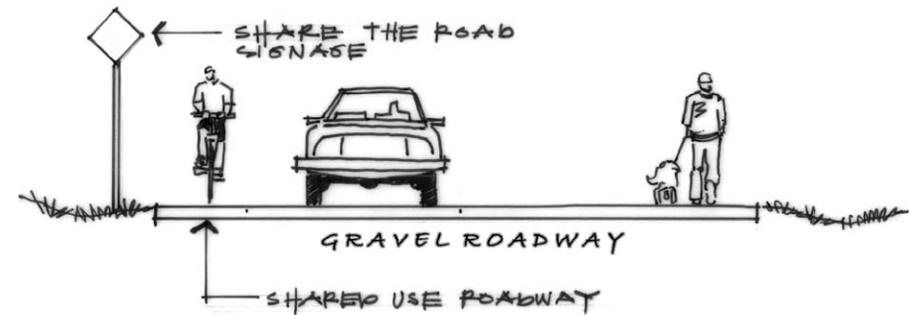
### Rest Areas and Overlooks

The system should include strategically placed rest areas and overlooks. This might consist of a single bench or more improved sites with rest rooms and other amenities. Generally these should be available within a mile of any point on the trail.

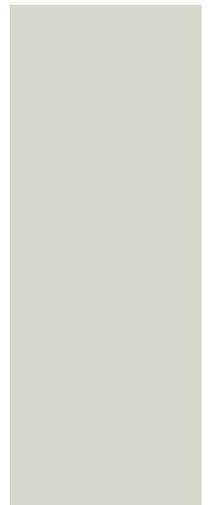


### “Backroad” Shared Use of Gravel Road Corridors

The Rangely area enjoys the benefit of a network of rural back roads. Most of these have low traffic volumes and speeds, are well maintained and wide enough to accommodate bike (wider tire hybrid and mountain bikes) and pedestrian use. This road network might function better with the addition of “share-the-road” yellow diamond traffic safety signs (per the U.S. Manual of Uniform Traffic Control Devices) and distinct wayfinding “mile markers” that help guide users from point-to-point. It would be helpful to place the mile markers with a ½-mile spacing in both directions and/or at least at any key decision points such as where the road forks and other intersections.



# CHAPTER THREE: RECOMMENDED LAYOUTS AND ALIGNMENTS



Overview: The Opportunity Areas and Corridors  
Center Loop: Stanolind Avenue to Camper Park  
West Loop: West of Stanolind Avenue  
South Loop: Connects all other loops to BLM lands  
College Loop: The end of Ridge Rd to Kennedy Dr  
East Loop: Kennedy Drive to Highway 139  
Future Study Area: A future North Loop along the White River

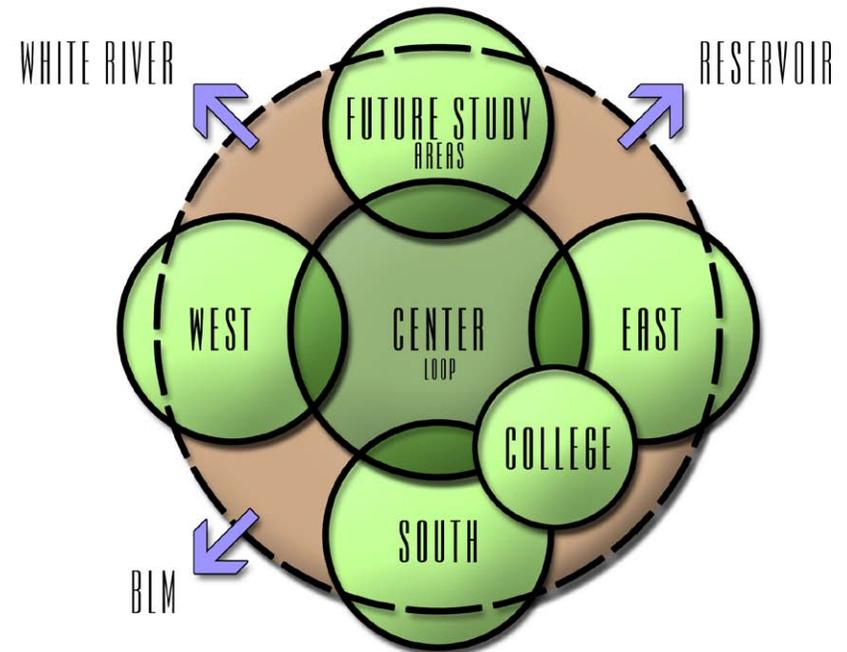
- Outlying Connections: North BLM lands and the trail to Kenney Reservoir

This chapter describes the specific recommended trail alignments, loops and corridors. It consists of an overall loop circumnavigating the entire town, and a series of smaller 'internal' loop trails. The trails are a mixture of off-road paved or gravel multi-use hike/bike paths, on-street shared-use routes, and "back roads" routes that access scenic areas and link key destinations. The "back roads" routes consist of unpaved, low traffic corridors crossing both private and public lands.

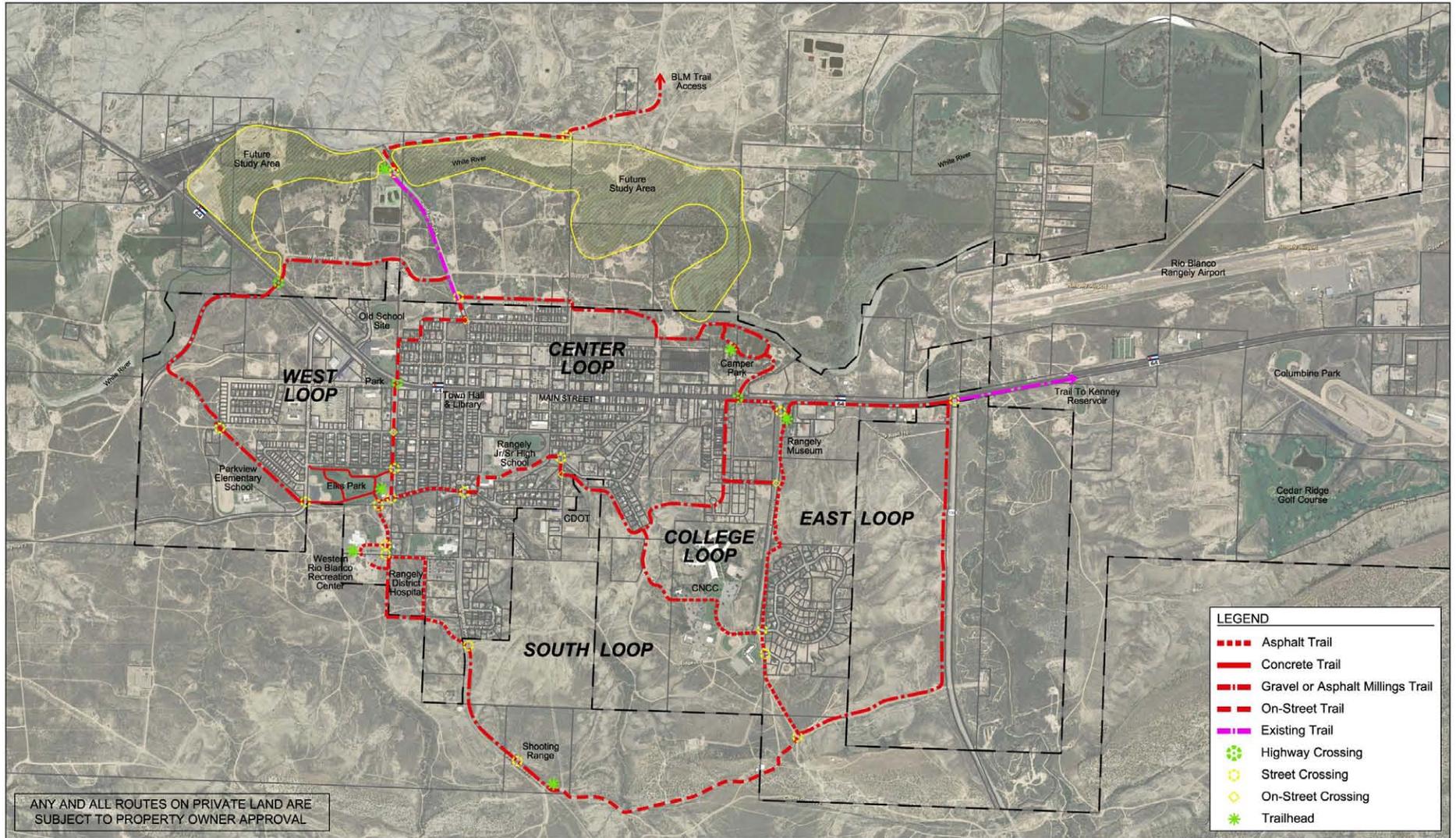
The core trail layout is presented in a series of five integrated loops. Each loop represents a logical trail link that can both stand on its own as a usable amenity and form part of the larger complete trail as additional segments are funded and constructed. With respect to the paved multi-use trail, the layout and alignment map shows an optimal route. The descriptions to follow describe the optimal routes, but interim solutions are also described in many instances that are lower in construction costs and may allow various loops to be realized prior to funding the optimal route.

#### Overview: The Opportunity Areas and Corridors

The study area for this plan is primarily within the Town limits and immediately adjacent to the Town. In evaluating this area, and with community input in public meetings and discussions with citizens and agency representatives, it became evident that there is a range of connectivity options and unique scenic/recreational opportunities to be taken into account. These elements include: the Northwestern Colorado Community College and the neighborhood to the east, the public elementary and middle / high schools, Elks Park, the Recreation Center, Camper Park and the White River access points, and BLM lands. These areas are integral to a vision of a larger complex of lands both privately and publicly held that could constitute a special legacy for the Town of Rangely.



Conceptual Diagram



The Integral Project Segments  
The above loops define logical, usable trail segments that can ultimately connect to complete a continuous trail loop around and through the Town of Rangely.

1. Center Loop: Stanolind Avenue to Camper Park
2. West Loop: West of Stanolind Avenue
3. South Loop: Connects all other loops to a trail through BLM lands

4. College Loop: The end of Ridge Road to Kennedy Drive
5. East Loop: Kennedy Drive to Highway 139

6. Future Study Area: A future North Loop along the White River
  - Outlying Connections: North BLM lands and the trail to Kenney Reservoir

### Loop 1: Center Loop

This loop begins at Camper Park trail head with parking and picnic facilities for day use as well as overnight camping facilities. A mini loop of 0.4 miles provides connections from the trail head to the primary Center Loop route as well as past an open play field and the scenic fishing ponds. This segment of the Center Loop provides the best opportunity for accessing the White River. River access points should be provided for swimming, tubing, rafting and fishing in this area.

The trail runs west then south from Camper Park to the Community Gardens then due west along the undeveloped 'alley' north of Rangely Ave. The trail then follows an existing social trail up to the southern edge of the drainage ditch along Denver Ave until reaching Weber Street when the trail switches to the north side of the ditch to avoid the development and traffic along Denver Ave.

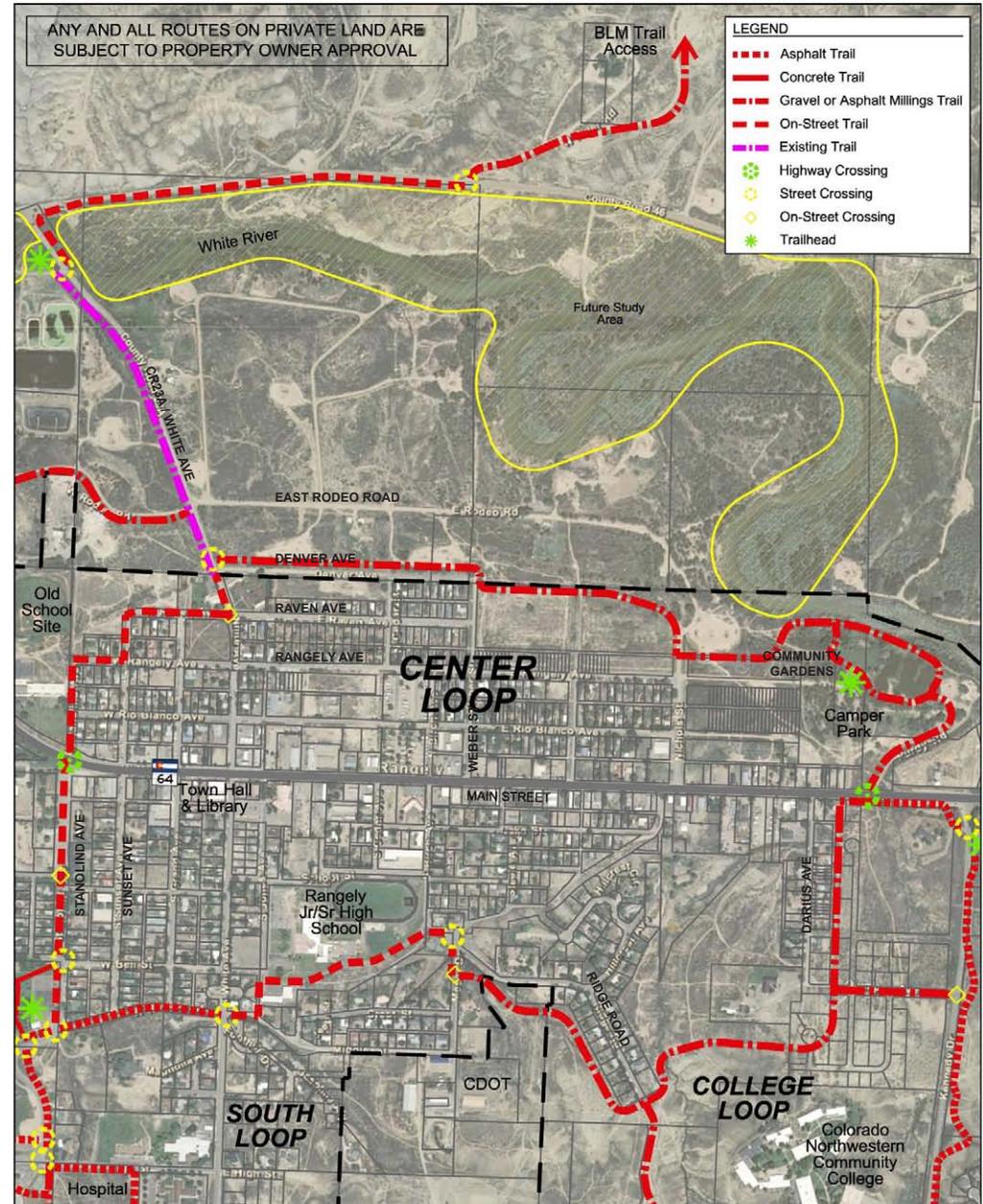
The intersection at County Road 23A and White Avenue is an important link to the West Loop as well as to the existing trail along the west side of the road, which connects to the second key trail head on the White River as well as access to the BLM lands north of the Town. This intersection is a low-volume road and must be well signed to alert trail users as well as approaching vehicles.

Once across County Road 23A the trail runs south briefly, sharing the road and then turning west on Raven Ave, to Sunset Ave, then Rangely Ave and turning due south at the old school site and following Stanolind Ave. These shared road segments will need to be well identified as the trail route and warning trail users and vehicles to share the road.

The trail crosses Main St/Hwy 64 and continues south on Stanolind. The highway crossing requires warning signs and crosswalk striping for trail users and motorists.

At the south end of Stanolind Ave the trail connects with the South Loop and the West Loop via the Elks Park Loop, and meets the Elks Park trail head with parking, picnic, and rest room facilities.

The Center Loop turns east along the north side of the Royden Ditch until reaching White Ave crossing, briefly following Middle Street then Jones Ave until reaching the west side of the high school. The trail runs briefly through a vehicle service area until reaching the emergency access road along the south edge of the school. Additional trail gates/barriers and signage are needed to limit vehicles in this area.



The trail connects via Birch Street and Mesa Drive to an alignment parallel to Hillcrest Ave and Ridge Rd. through privately owned land. If an easement through this private land can not be achieved, then the trail may remain on street until the end of Ridge Rd. where the Center Loop joins the College Loop. The trail heads east through a steep draw following an existing social trail that will need signage and trail gates to limit use by ATVs.

The trail crosses north through Town owned property east of the homes on Darius Ave until reaching the existing trail on Main St/Hwy 64. The best location to cross the highway is at Purdy Road, a park service road and the final connection back to Camper Park closing the 3.6 mile long Center Loop Trail.



Camper Park Trailhead Location



Community Gardens



Alternate Trail Location North of Community Gardens

### Loop 2: West Loop

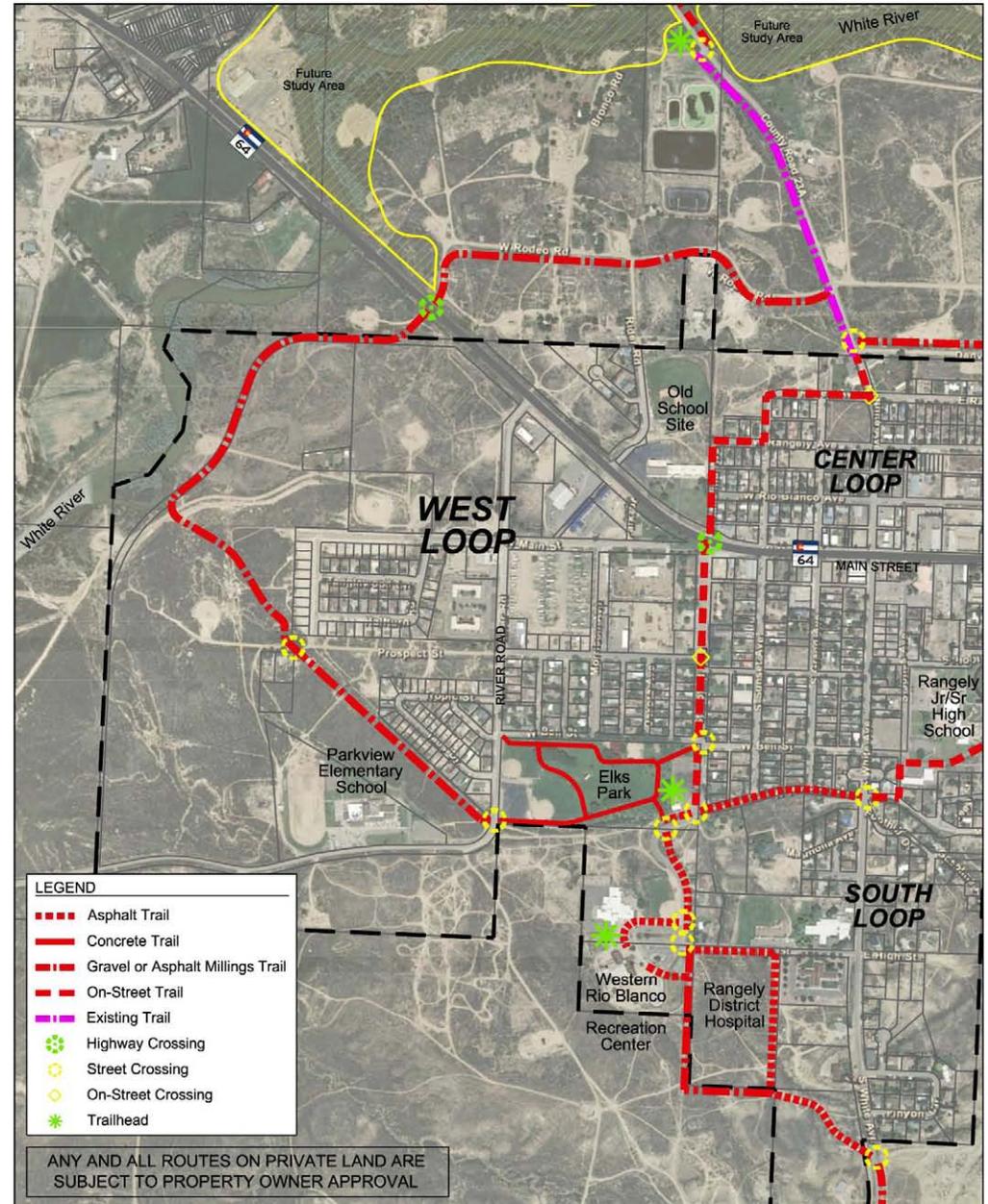
This loop shares the western most segment of the Center Loop and begins at County Road 23A / White Ave off the existing trail along that road. The trail heads west along Rodeo Rd, a very low volume dirt road that can be shared with the trail until eventually a separate trail can be established along the south side of the road. The trail curves briefly south to align with an existing intersection at Highway 64. Signage and crosswalk striping will be needed to warn trail users and motorists of this crossing point.

After crossing the highway the trail heads west then south through Town owned open space, Town easements and a possible future park development area along the White River. Trail crossings at well access roads may need signage to warn trail users of possible vehicle conflicts.

The trail heads southeast through a challenging open area heavily cut with drainage channels. Keeping the trail to the northeast of the primary drainage channel will be the most stable land and require fewer drainage crossings.

At the intersection of Prospect Street the trail aligns to the southwest side, and along the top of the berm of the drainage ditch that heads on a diagonal toward River Road and connecting to the eastern edge of Parkview Elementary School.

Crossing River Road the trail proceeds due east along the 'top' of the northeast side of Royden Ditch, south of the baseball fields. The



proposed surface of this section of the trail is concrete and ties into a concrete and gravel loop trail in Elks Park and connecting the residential neighborhood north of the park. The trail meanders through the mature trees in Elks Park and connects with the trail head with parking, picnicking and rest room facilities on the east edge of the Park.

This is the connection to the Center Loop and the South Loop. Turning north along Stanolind Ave (the west side of Center Loop) returns trail users to the beginning of the West Loop for a total distance of 2.7 miles.



Stanolind Avenue - Looking South



Elks Park - View from Royden Ditch



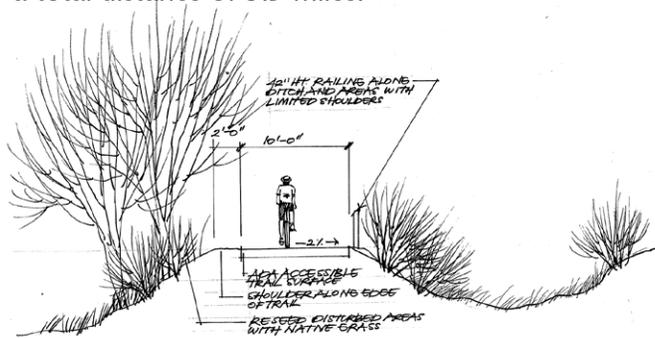
Royden Ditch Trail at Elks Park - Looking East

### Loop 3: South Loop

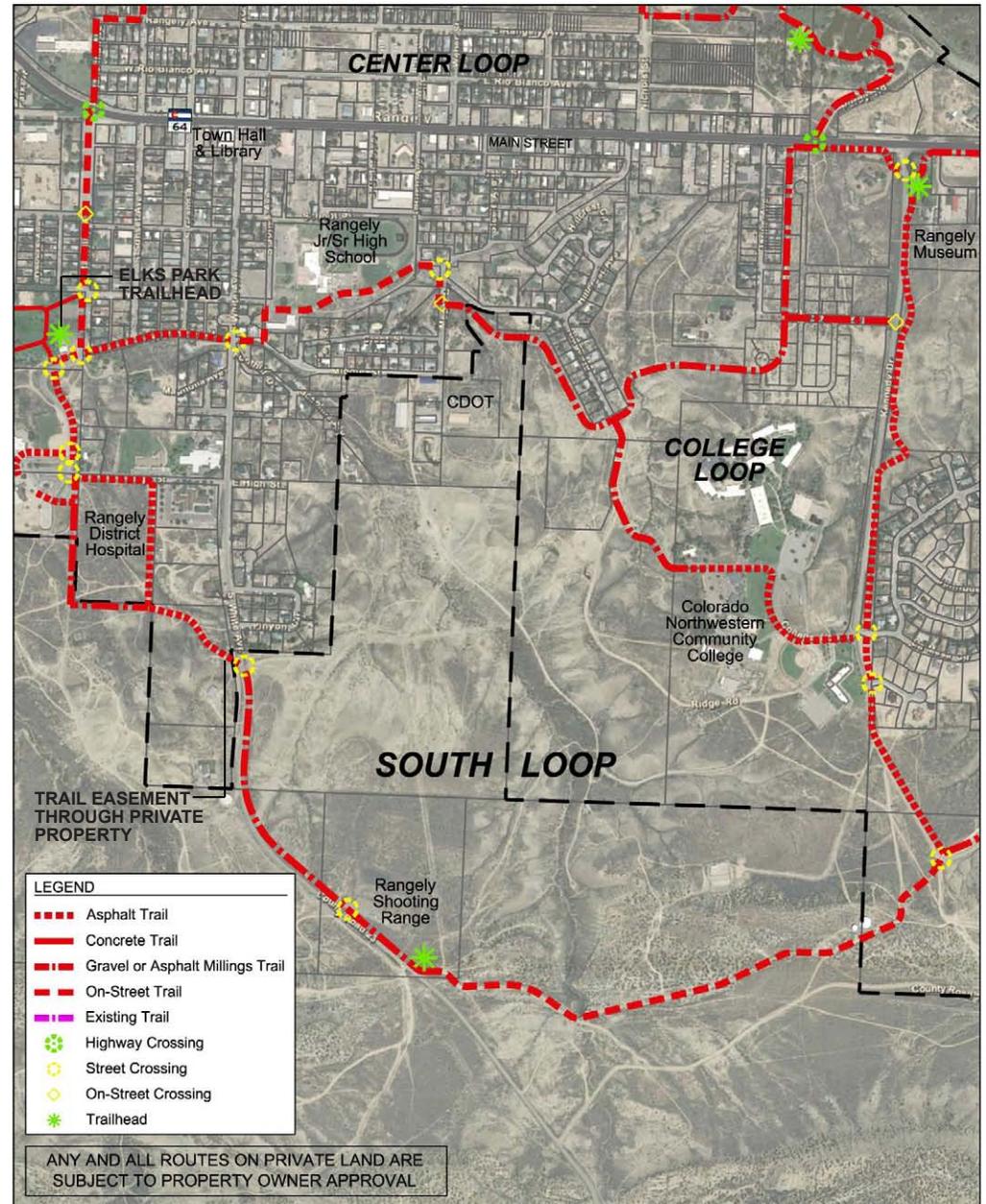
The South Loop begins at the Elks Park trail head and crosses the Royden Ditch at White Ave and/or over a separate trail bridge west of White Ave, then crossing Stanolind Ave and connecting to the existing trail up to the old elementary school site and the Western Rio Blanco Recreation Center. The existing asphalt trail will need to be widened and extended to connect to the old school property and the Recreation Center. The old school site provides opportunity for additional park and recreational opportunities ideally situated for residents and visitors to the Town. There is also an opportunity for another trail head in this area.

The trail leaves the Recreation Center heading southeast to the proposed Rangely District Hospital property and will circumnavigate around the edge of this property and connect into a trail easement connecting the hospital property to White Ave.

White Ave becomes Big Park Road/County Road 23. The trail will be gravel or asphalt millings parallel to, but separated from, the road until reaching the south side of the Rangely Shooting Range where the trail turns onto an existing primitive trail used by mountain bikes and ATVs. This segment of trail through the BLM land will remain rugged and challenging, ideal for mountain biking enthusiasts. This section of trail eventually connects to the East Loop trail, heads north to the southern segment of the College Loop trail, then west on the southern segment of the Center Loop trail and finally reconnecting at Elks Park for a total distance of 3.9 miles.



Section - Royden Ditch





BLM Land on South Loop



Bridge Location at Royden Ditch



Existing Trail to Recreation Center

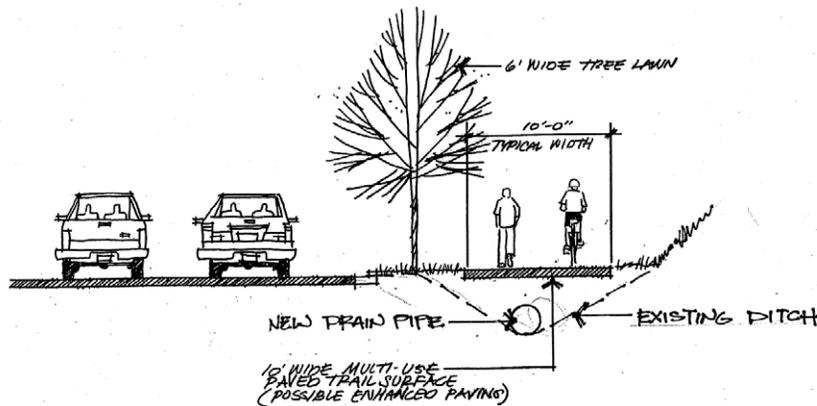


BLM Shared Road / Trail

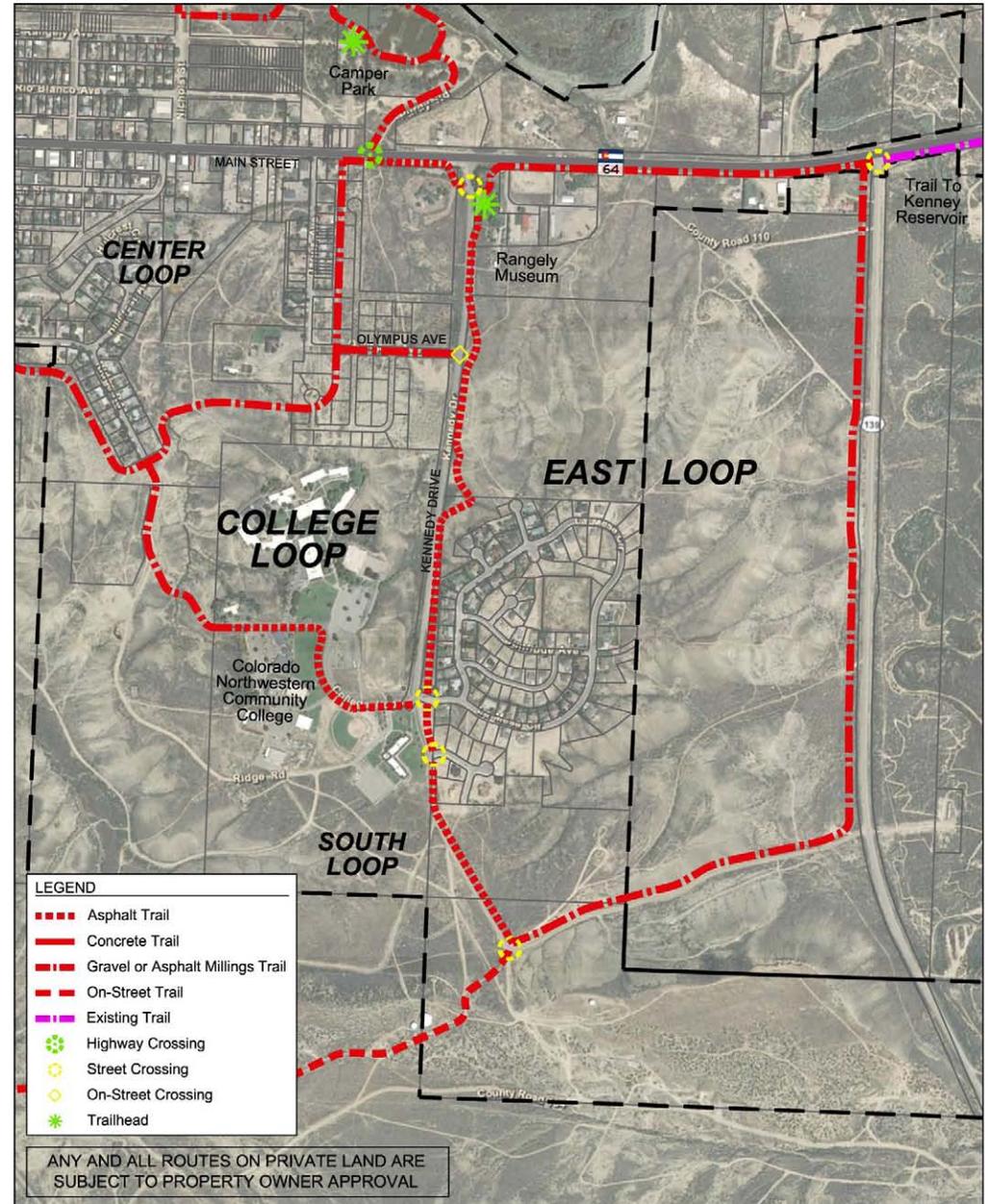
### Loop 4: College Loop

The College loop shares segments with the surrounding Center Loop, South Loop and East Loop providing excellent connectivity between the Community College and the surrounding areas of Rangely. Sharing a segment of the South Loop beginning at Ridge Road and heading East, the trail runs through the center of the college campus along College Rd to the intersection at Kennedy Dr where the trail shares a portion of the East loop going north and down the steep incline of Kennedy Dr. The trail turns west at Olympus Ave or can be extended further north to Main Street, then west. In either case, the trail joins the southeastern edge of the Center Loop and swings west to Ridge Road where it connects to the northeastern portion of the South Loop and returns to the college for a distance of 1.6 miles using the Main Street route or 0.8 miles using Olympus Ave.

The College Loop strategically connects all the other loops while providing improved access to the Community College population. The final alignment through the College should be defined by CNCC Facilities Management in conjunction with the 2009 Facilities Master Plan and any subsequent updates.



Section - Typical Access Road Section





Trail at End of Ridge Road



Trail Crossing Kennedy Drive Near Main Street



Trail Connection at Main Street

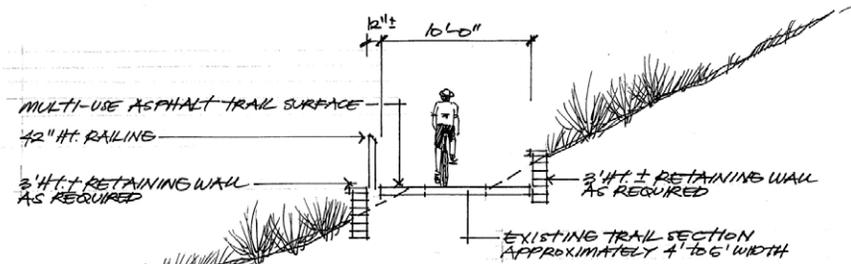


Trail Connection at Kennedy Drive

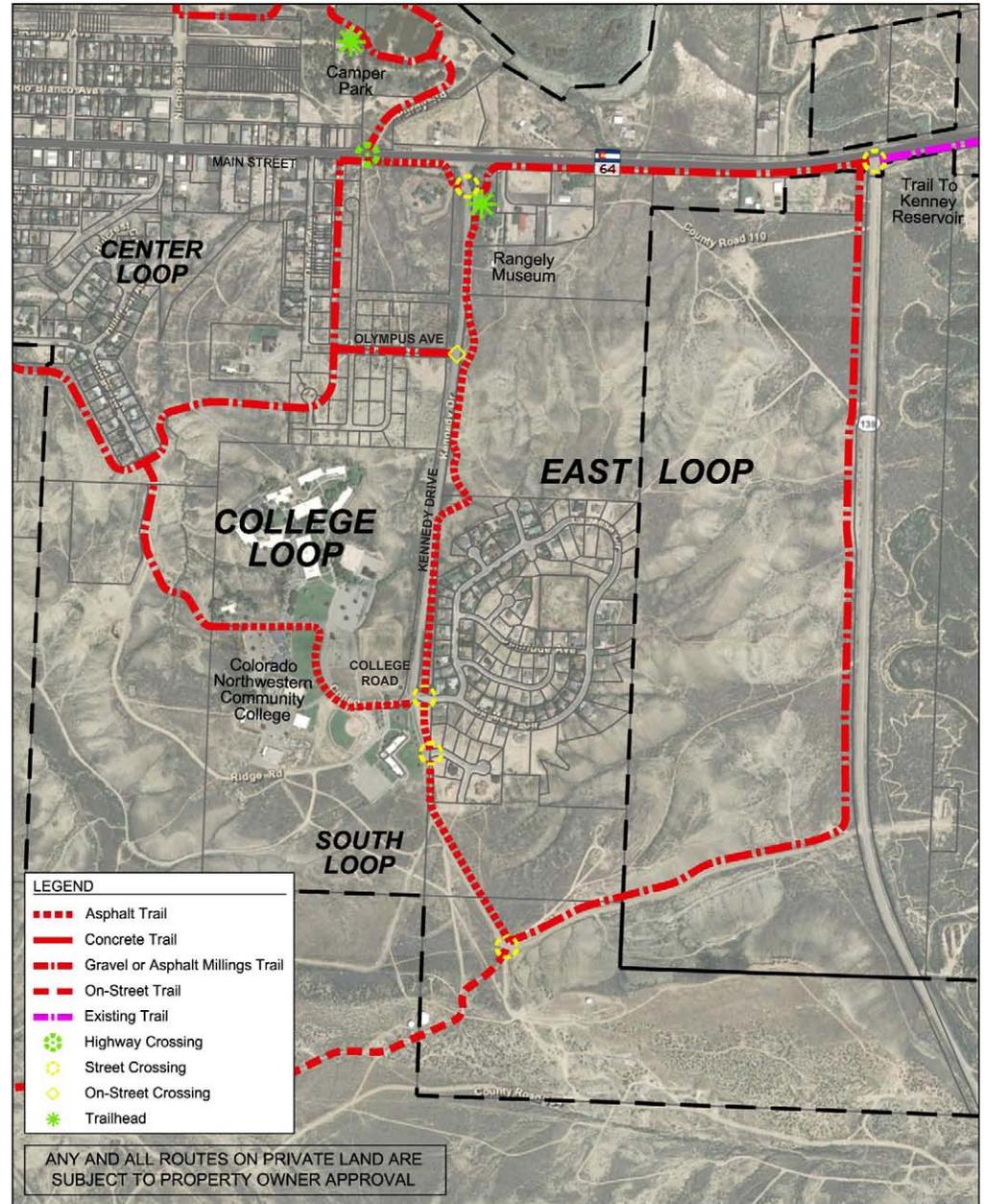
**Loop 5: East Loop**

A key segment of the East Loop is along Kennedy Drive and its proposed that this segment should be widened to provide a safer off road trail experience. This segment of the trail slopes steeply to the north, but receives a high degree of use from the Community College population and the residential community to the east. In addition to widening, lighting (of the trail) would also aid in creating a safer route along Kennedy Drive. Lighting of this trail is dependent on funding. At the intersection of College Rd, the East Loop shares a segment of the South Loop onto BLM lands until reaching the eastern most point of the South Loop and turning east following a BLM access road until reaching Highway 139.

The trail swings north along the western edge of 139, continuing past very dramatic open lands until reaching Highway 64 and the existing asphalt millings trail along HWY 64 that travels to Kenney Reservoir to the east or back into town to the west. The East Loop connects back to Kennedy Drive at the Rangely Museum, a proposed location for a trail head. At this location, trail users can connect to the Center Loop or continue south up Kennedy Drive to complete the 2.8 mile East Loop.



Section - Kennedy Drive





Trail to Kenney Reservoir



Trail Along Kennedy Drive Shoulder



Trailhead Connection at Rangely Museum to Kenney Reservoir



Steep Terrain Along Kennedy Drive Trail

### The “Back Roads” Network

In addition to the proposed Loop Trails there is an opportunity to enhance a network of scenic “back road” corridors and single track mountain bike trails. These low traffic, low speed, gravel roads and dirt trails allow opportunities for enjoyable bicycling and walking through undeveloped BLM lands to the south and north of the primary loop trails.

An example of this kind of network is the segment of trail extending north of Town along County Road 23A, crosses the White River and then briefly shares a section of County Road 46 before heading north into BLM lands. This area may be of particular interest to more experienced mountain bike enthusiasts.

The South Loop also provides access to many “back road” trails on BLM lands and tends to be somewhat flatter and less challenging, suitable for less experienced mountain bikers. Both the south and north “back road” trails are shared with off-road vehicles including ATVs and warning should be posted for all users to yield to slower traffic and be courteous to fellow recreation enthusiasts.

It should be noted that the “back road” network includes areas with steeper grades and longer distances to travel - so they may not be optimal for all users. Nonetheless they offer an opportunity to create an exceptional and unique recreational and fitness network serving both residents and visitors to the area. Much of this “back roads” network is already in place and signed although additional mapping and wayfinding markers could enhance and facilitate use of these trails.

### Access Points

Improved access points are recommended at White River/White Ave, Elks Park, Western Rio Blanco Recreation Center, Rangely Shooting Range, Rangely Museum and Camper Park. These access points/trailheads should include an entry marker or monument identifying the trail and a system map/wayfinding display. At major access points at Elks Park and Camper Park entry markers shall include user courtesy/trail regulations and other relevant information.

### The Future Study Area

The White River is an under utilized recreational opportunity for the Town of Rangely and surrounding residents. The river meanders along the north edge of the town for a short distance, but is mostly within County and Federal lands that the Town does not control. Unfortunately, a good deal of this land has been given over to gas wells and associated support facilities, some of which is out of service and in an unsightly condition of decline and would pose a safety hazard to the general public. The goal should be to eventually clean up this important resource and wildlife habitat for the enjoyment of the residents of Rangely and Rio Blanco County as well as tourists and visitors to the area.



### Alternate Trail Surface Recommendations

For purposes of this report, trail layout and surface recommendations considered the proposed trail network to be separated from the roads. Trail pavement has been identified as asphalt millings along a majority of the road system. This pavement type and alignment is planned for as the most ideal condition for the trail system and is the ultimate goal for the trail system. The pavement cost can be reduced and offset through use of in-kind labor and donations of material. Other short-term, immediate considerations for cost savings can look at planning for the trails along W. Rodeo Road, S. White Avenue, the BLM access roads and Highway 139 as shared use trails with the dirt and gravel roads. These are identified as the “Backroad” trails for Shared Use of Gravel Road Corridors in Chapter 2, Design Guidelines and Principles.

# CHAPTER FOUR: IMPLEMENTATION



Organizational Structure for Effective Implementation:  
Community Participation  
Rights-of-Way and Permitting  
Phasing and Next Steps  
Fundraising and Budget Strategy  
Operations and Maintenance Considerations

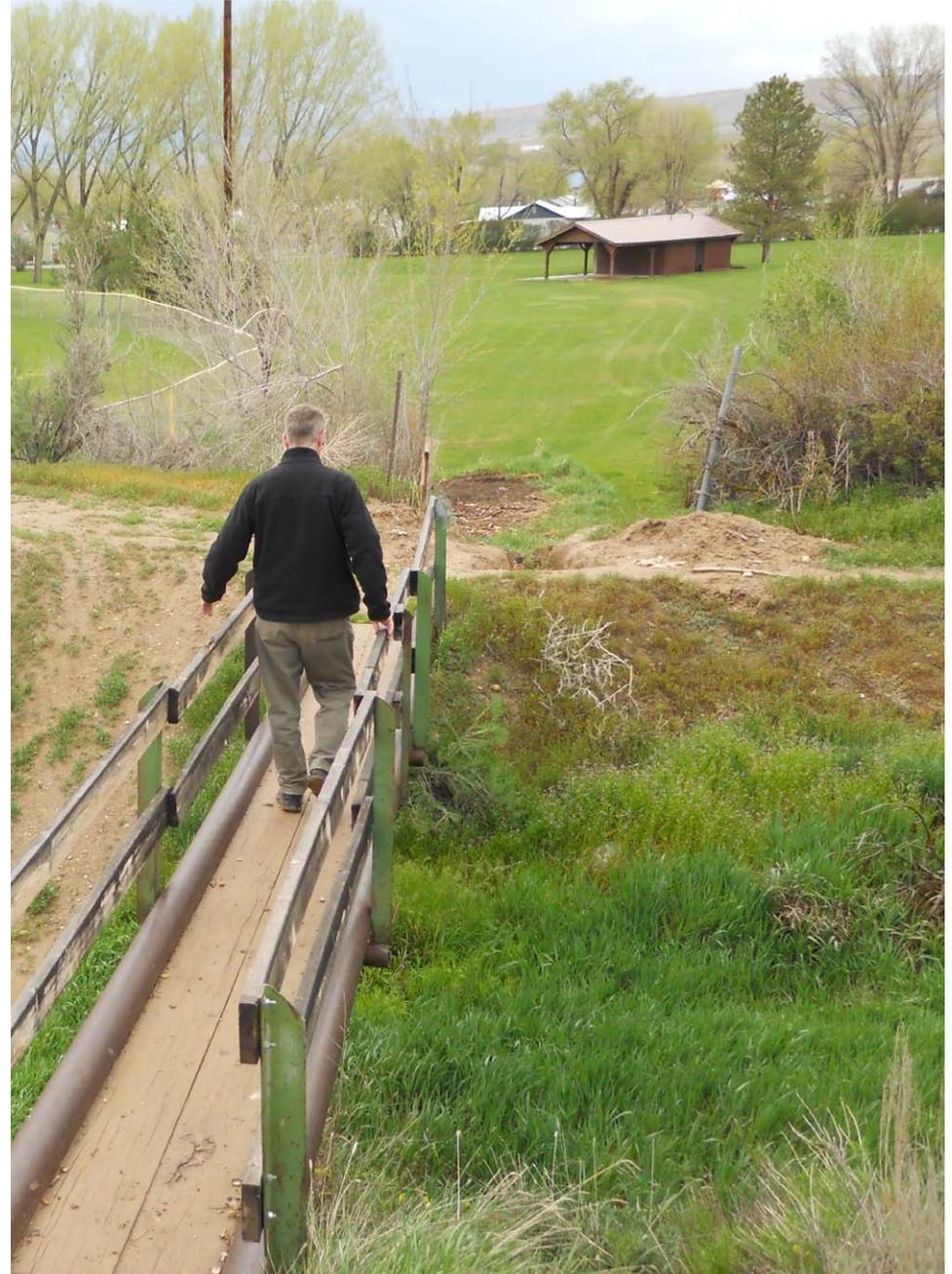
### Organizational Structure for Effective Implementation

The Town of Rangely Recreation Trails will become an important asset to the Town as segments are completed. Numerous studies have shown that green infrastructure—especially trails—are vital, supporting both livability and economic development. The trails can also become the center point for a larger regional network of trails and scenic values shaping the character and quality of Rangely and Rio Blanco County area for generations to come.

Many leading mountain and resort communities in Colorado and around the nation have recognized this and have invested over the past several decades in these kinds of improvements. The elements laid out in this report also offer a vital step in expanding the existing trails that the Town has established along Kennedy Dr, CR 23A access to the White River and SH 64 access to Kenney Reservoir helping Rangely and Rio Blanco County compete as a world-class place to live and do business.

The success of the vision in this plan requires timely implementation of the components. Starting in 2012, and each year thereafter, a “ribbon should be cut” on logical, usable and inspiring trail segments and conservation agreements. This calls for putting in place an effective and enduring organizational structure providing leadership and strong community engagement. There will be a need to forge a cooperative effort among the parties to see all of the improvements through to completion. Skills in community advocacy, working with property owners, design, engineering, right-of-way acquisition, fund raising and overall coordination will be needed. Staff, consultants, political leaders and other champions for the project must be engaged in the process.

A key to this is having a designated entity and “point person” charged with managing the project and accomplishing completion of the project segments on time and within budget. There also needs to be a designated entity with authority to accept grants, appropriate funds, accept right-of-way conveyances, retain contractors, monitor construction and take on long-term operations and maintenance.

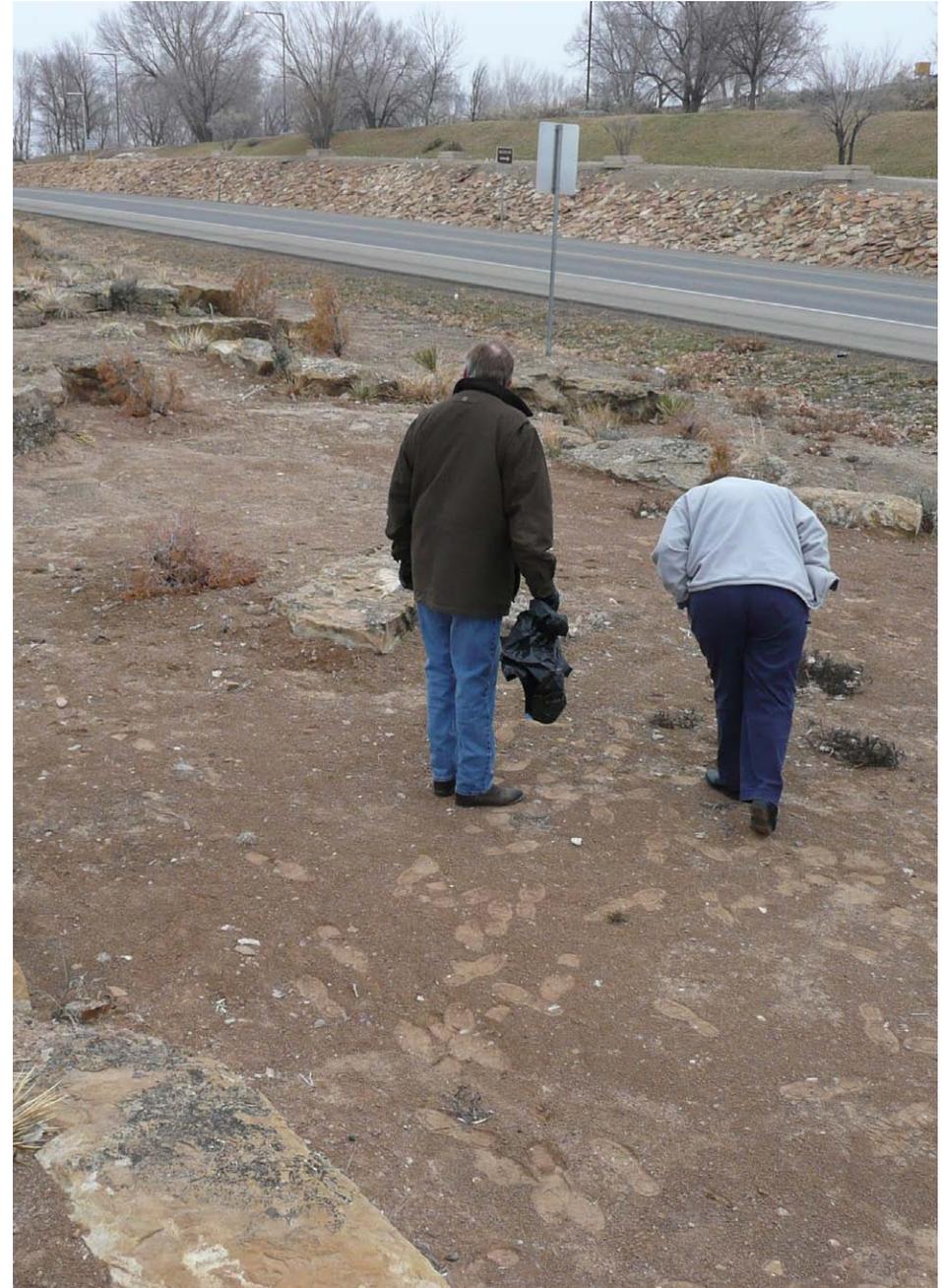


There are several models for accomplishing this, ranging from public agency management to private sector leadership to a public/private partnership. The Town of Rangely is fortunate to have an engaged, articulate population and a willingness of residents and business people to commit time and resources to completing the trail system. This was clearly evident in the Key Stakeholder meeting when participants shared their ideas and enthusiasm. It was also impressive to see the skills and commitment of the staff and elected officials in the process. At this point, the community does not have an existing private entity such as a non-profit, but an upcoming task could be creating an entity or engaging an existing entity in the region to help champion the trail.

One option is a public/private partnership that could be put in place. This effort should be led by a coalition of Town staff representatives working in coordination with the County and a citizens or “friends of the trail” group. Initially, the “friends” group could be informal, but there is value in the group evolving into an incorporated non-profit with tax-exempt status under Section 501 (c)(3) of the U.S. Internal Revenue Code. This would allow the group to accept private donations, possibly hold conservation easements and provide other services to the effort. It is strongly recommended that the “friends” group focus strictly on completing the improvements outlined in this report—as opposed to having multiple activities and missions—until the work is largely completed.

There are a number of excellent models of this in Colorado and around the nation including: The South Suburban Park Foundation, Inc. in Littleton, CO, The Poudre River Trail Corridor, Inc. in Larimer and Weld Counties, CO and the Platte Parkway Trust in Casper, WY. Durango also has an example of strong leadership in this area.

In this scenario, the Town and County key representatives form a “leadership council” consisting of key agency staff and elected officials to coordinate project management and development activities, each responsible for work in their respective jurisdictions. This group could be led by Project Coordinator(s) to chair the overall effort—one from the County and one from the Town. These officials lead the development





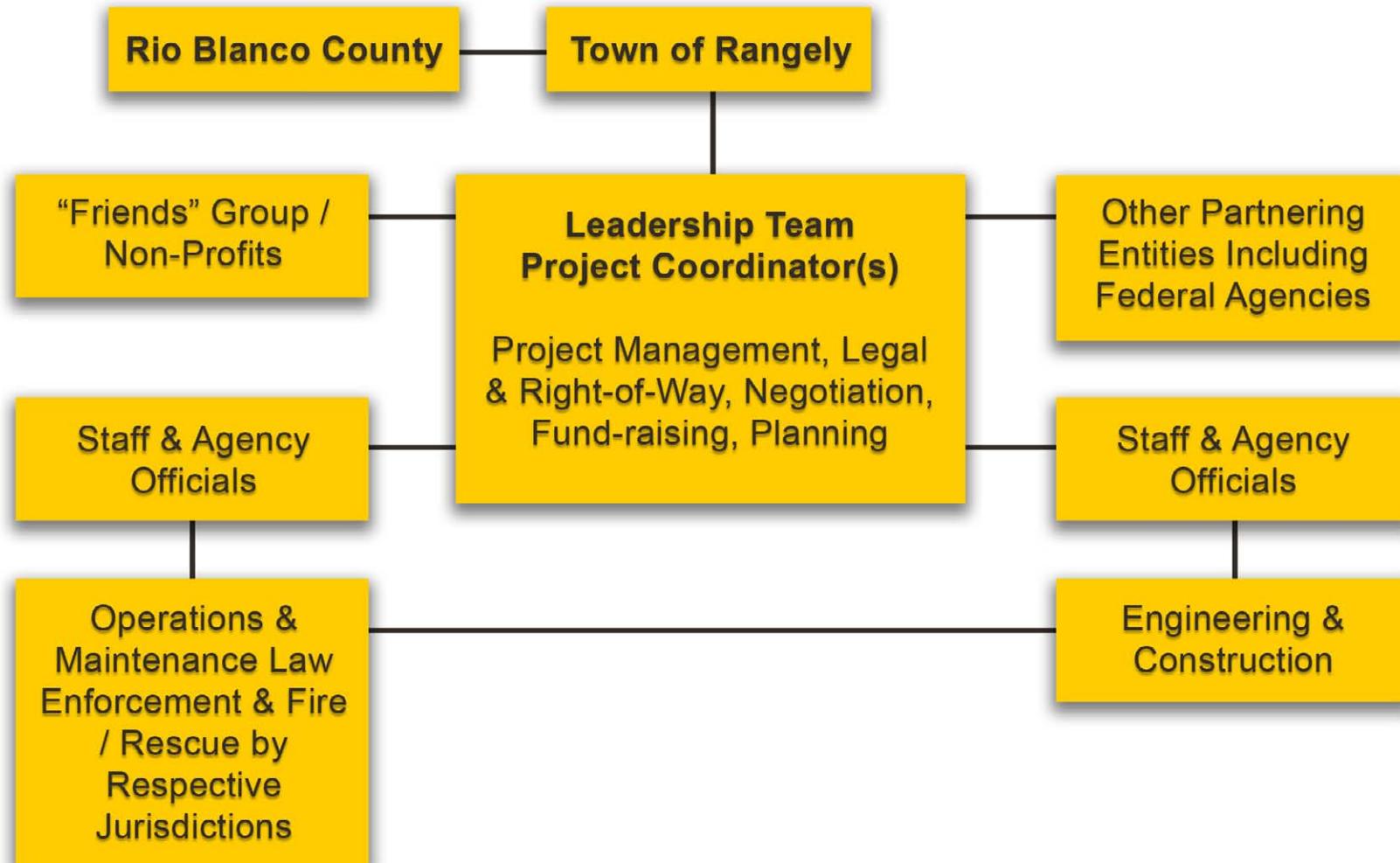
process, providing staff services to pursue rights-of-way, raise funds, coordinate planning, design and construction, and otherwise oversee and lead key implementation activities. This, of course, is carried out in close cooperation with the other partnering entities—particularly the “friends” group.

In most cases, as appropriate, depending on funding sources and other considerations, the respective public agencies take on responsibility for contracting and supervising construction services and engineering. The respective jurisdictions, in cooperation with the partners and the “friends” group oversee operations and maintenance. The appropriate police, fire and rescue authorities will have responsibility for law enforcement and security along the trail and on associated properties.

At some point if the “friends” group decides to formalize its structure, this organization might decide to contract with a professional “Project Developer” who would take on leading the implementation effort. This partnering structure could evolve through an intergovernmental agreement—formal or informal—amongst the entities and in this instance the governmental entities would contribute all or part of the funding to hire development services. This model has worked quite successfully in the South Suburban example in Metro Denver and other places. Initially, however, the jurisdictions need to commit the staff and resources to manage the effort.

Regardless of the structure, it is essential that all activities be carefully coordinated through the designated lead entity and a key individual(s)—the Project Coordinator(s). This is especially important as the project involves negotiations with private property owners, applying and administering state and federal grants, public information and other activities that call for the group speaking with a singular voice. The following table might best describe the management structure and working relationships.

## Implementation Organizational Framework





### Community Involvement

This trail plan included a key stakeholder participation process. Area residents, trail recreationalists, schools, businesses and others all have a stake in the outcome. These individuals and entities should be kept well abreast of progress on the trail in a timely manner and their input openly received and responded to. The Project Coordinators should keep a list of the contacts and regularly brief them. This might also include holding public update meetings at key junctures in the implementation process.



### Rights-of-Way and Permitting

To expedite the process—and because right-of-way and permitting can be a lengthy process—right-of-way and permitting efforts should continue expeditiously. In addition to trail and conservation easements on willing private properties, right-of-way license agreements will likely be required from CDOT in certain areas. Permits including possible approvals under Sec 404 of the Clean Water Act might be required for any trail work that impacts wetlands.

Typically, easements provide the best acquisition instrument. Easements are a permanent land ownership instrument that does not include the entire property—just the right-of-way to a trail and any associated landscaping and/or conservation areas. The process will require surveying, legal descriptions, negotiation, appraisals and legal services. If outside funding is involved, such as federal transportation enhancement funds, more stringent steps may be required and must be carefully adhered to. While there are no known environmental hazards (i.e. contaminants) proper due diligence is always recommended that may include a site evaluation by a qualified environmental consultant.





### Phasing and Next Steps

A conceptual cost estimate was prepared as part of this plan. Please see Appendix "A". It is, however, a "planning level" estimate useful for initial budgeting, phasing and fund-raising. It is anticipated that design consultants will prepare more detailed cost estimates during the construction phase. Based on available funding and other considerations, it is suggested the project be divided into several phases and funds budgeted with the goal of completing a logically usable phase (which can stand on its own) each year.

### PROJECT PHASING AND NEXT STEPS

**Phasing of projects is best guided by several criteria including:**

- An immediate opportunity where a logical, usable connection can be made with current or readily available resources such as Kennedy Drive trail, Royden Ditch trail and Elks Park loop trail.
- Availability of rights-of-way and permitting.
- Availability of funding and/or grants to build and maintain improvements
- Catalytic projects that demonstrate the value of the project, build public support and help promote further community support and fund-raising
- Opportunity to include a trail with a current private or public development project such as a highway improvement, access road or new subdivision.
- Projects that can be completed using volunteers or in-kind labor and resources.
- Segments that offer an exceptional experience and/or are highly visible to the public.

In strategizing implementation, several early action projects were defined. The goal is to complete these in the next 1-3 years. Based on the above criteria field conditions, cost and land ownership considerations, the key logical phases appear to be:

### Kennedy Drive Trail Improvements

- Reconstruct the existing trail along Kennedy Drive from Main Street to the Community College. This is a vital link to the College and the adjacent neighborhood, but the existing trail is too narrow and presents safety issues. Currently a length of the trail is shared with vehicles on Kennedy Drive. The proposed trail reconstruction rectifies both of these issues as well as proposing lighting since a great deal of the trail usage is at dusk, early morning and the nighttime hours.

### Royden Ditch Trail

- In conjunction with the trail through Elks Park, the Royden Ditch segment would provide a trail between the High School and the Elementary School and the residential neighborhood between the schools. This trail segment should be relatively inexpensive to build yet provide great 'bang for the buck' providing a safe and convenient connection between two key community assets.

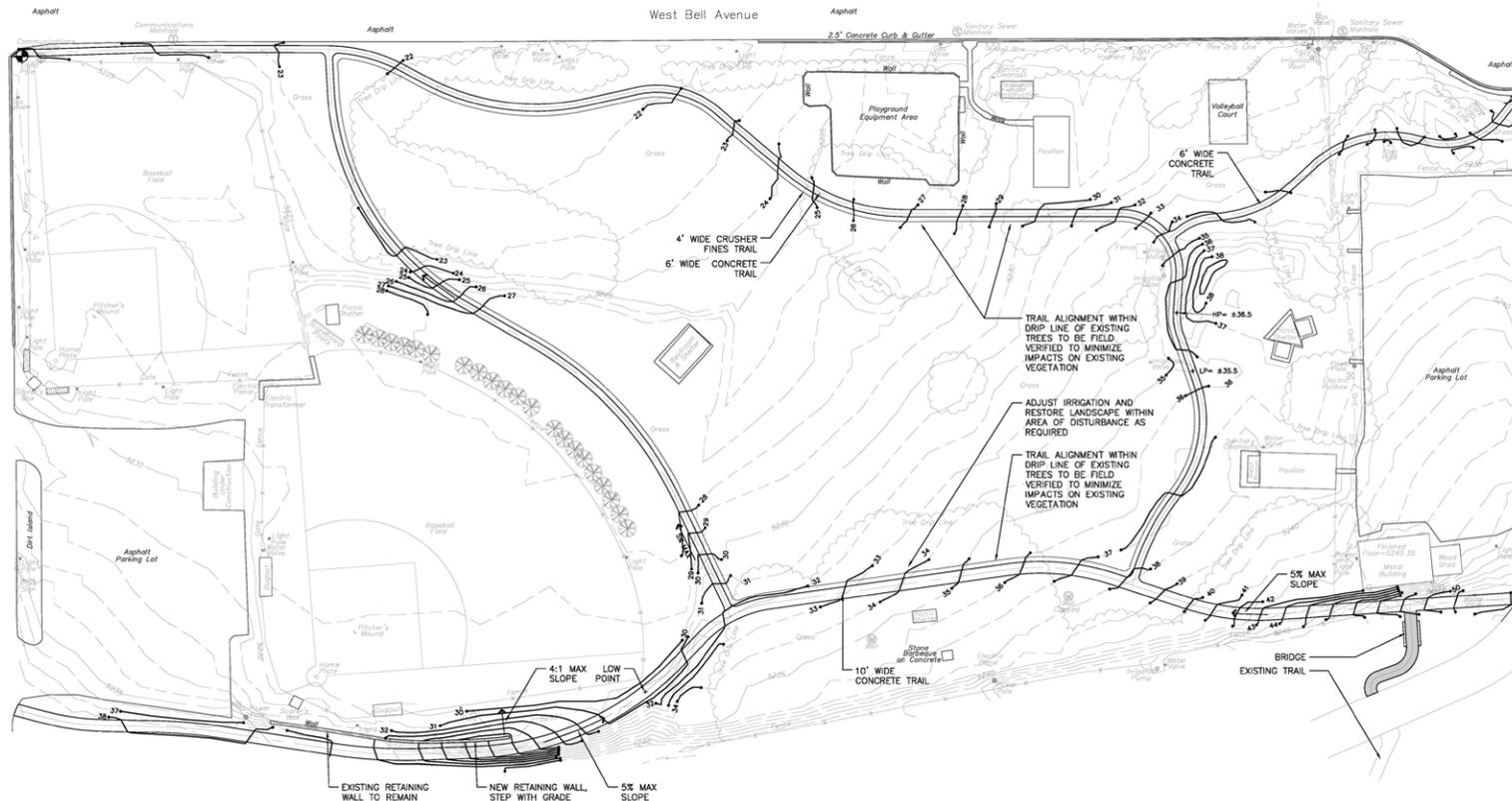


### Elks Park Loop Trail

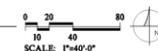
- Elks Park is a beloved and well used community park for Rangely and Rio Blanco County. This master plan proposes a small 'loop' trail within the park, a part of which is also a segment of the West Loop trail. Elks Park is also an important trail head at the confluence of the West Loop, Center Loop and South Loop. In conjunction with the Royden Ditch trail segment, this segment of the West Loop will provide a safe and convenient trail connection between the High School and the Elementary School along with the residential neighborhood between those two community assets. The Elks Park trail segment of the West Loop and the small internal park loop will be highly visible to the community and will be well used and appreciated.

### Elks Park to the Western Rio Blanco Recreation Center

- This short section of trail provides an important link between Elks Park and the Recreation Center. Ideally, a short bridge is used to cross the Royden Ditch and provide direct access to the existing trail up to the recreation center. An interim plan could route trail users from the south edge of the park, east to Stanolind Ave to cross the ditch on the street, then proceed west to connect with the existing trail up to the recreation center. A bit circuitous, but certainly an acceptable option. The existing asphalt trail up to the recreation center will eventually need to be widened and most importantly needs to be extended up to the recreation center. The old school site provides additional recreational opportunities for residents or perhaps tourist based attractions such as a year-around adventure park.



ELKS PARK TRAIL - PLAN



### Next Steps

There are several actions that can and should be taken immediately to initiate moving beyond this plan into concrete actions. These include:

1. Identify the key staff project coordinators to continue implementation activities.
2. Engage elected officials in the plan and move toward timely adoption of the plan.
3. Refine a schedule and “roster of projects” for logical phased implementation of the full trail plan. Develop a cost estimate and a realizable right-of-way acquisition plan for a 2012 or 2013 pilot project.
4. Convene representatives of the jurisdictions and “friends of the trail” to agree with and assemble an organizational structure.
5. Work to solidify a “friends group” to promote the trail project and build liaisons with key stakeholders such as business people, land owners, developers and public spirited citizens who might be willing to contribute to the effort.
6. Create a public information program including a Web site to keep community posted on the plans, accomplishments, volunteer activities, etc. Consider developing a trail logo and wayfinding marker.
7. Immediately pursue negotiation of rights-of-way along the corridor.
8. Pursue state, and if available, federal funding including GOCO, State Trails funding in the next grant rounds.
9. Explore potentials for a long-term funding source such as an open space sales tax.
10. Review current development regulations and identify ways to improve ordinances such as, open space and trail corridor dedications, set-back requirements along roads to leave room for trails, and incentives to promote trail and greenway benefits.



### Funding Strategy

There are several potential funding sources likely to be available over the next several years. These include:

*Local Appropriations* - These funds come from Town and County revenues (property taxes, restaurant and lodging taxes) and other sources and appropriations. This local commitment is key to providing match money and helping leverage potential outside funding.

*Create a Designated Fund* - There are numerous examples of special taxes, typically a sales tax increment at the County level, dedicated to open space, trails, parks and other similar community investments. These projects have been phenomenally successful along the Colorado Front Range, in Summit County and other locations where a very small increment on sales can generate substantial funds that can be invested in infrastructure vital to the long-term health and economic well being of the community. This type of tax is especially beneficial in tourism-based places where a substantial portion of the revenue comes from outside the area such as a lodging tax.

*Local Development* - As new development comes on line a number of communities have been successful in funding trails and greenway improvements through the sub-division process. As an example, Commerce City, Colorado has been extremely successful working with developers to set aside open space corridors and build trails and parks in the developing area west of Denver International Airport. A key to this success was having a sound master plan in place. However, with the slowing of real estate development, less money is available for trail construction from these sources in the near term, though there may be opportunities for land and right-of-way designation on lands zoned for mixed use development. It should be noted that many developers want these improvements because they recognize them as essential benefits for the future marketability of their lands.

*Conservation Set-Aside Tax Benefits* - Under Colorado Law, landowners who agree to set aside developable lands for conservation purposes can reap substantial tax benefits directly, or cash benefits, by re-marketing the tax benefits to others in need of a state tax deduction.



The Southwest Land Alliance is an outstanding resource with a strong history of land conservation success in Archuleta County.

*Great Outdoors Colorado and the Conservation Trust Fund*—This program generates substantial funding each year both in terms of per capita annual distributions to jurisdictions and grant programs including the State Trails Program and other GOCO grant programs such as large scale and special opportunity grants. The latter may be a strong potential funding source for this project.

*Federal Funding* - Though the near-term and long-term future of these programs may be in doubt due to cutbacks, there are a number of potential federal programs that could become available. These include transportation enhancement monies; trail monies; Land and Water Conservation funds; “ARRA Stimulus” –type programs; health and fitness and community development programs. It may be helpful to monitor federal web sites to identify programs. American Trails ([www.americantrails.org](http://www.americantrails.org)) typically posts alerts about various programs. Possible time delays or more stringent grant terms and requirements and associated administrative costs should be weighed in considering certain federal programs. The benefit of larger grants (\$500,000 or more) and lower matching requirements may offset the higher application and administrative costs of some programs.

*Individual, Philanthropic and Corporate Giving* - There are several possible sources of private sector funding for trail projects. Programs and levels of sponsorship vary. Here, the “Friends Group” may be in the best position to seek and accept funds from private donors. These might include: gifts, grants, bequests, fund-raising events and other forms of giving.

*In-Kind and Volunteerism* - There are opportunities to engage in-kind services from public agencies or private participants both in land donations and possibly use of equipment, labor or materials. This might also include youth programs, scout projects and volunteerism.



### Operations and Maintenance Considerations

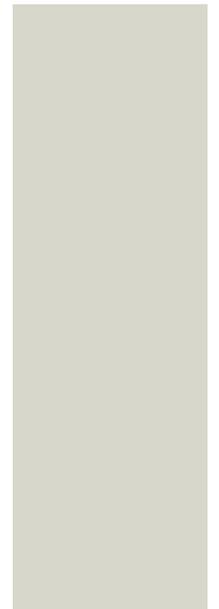
Key maintenance activities will include:

- Trail maintenance patrol and monitoring
- Snow removal as feasible and appropriate
- Trail sweeping
- Trail corridor weed and vegetation management
- Trail surface, fixture and furnishings routine repair
- Watering trees and landscape materials
- Application of fertilizer and pest management
- Litter and debris removal
- Remedial repair of improvements such as fixing washouts, erosion
- Public safety and rescue patrol, enforcement and emergency services

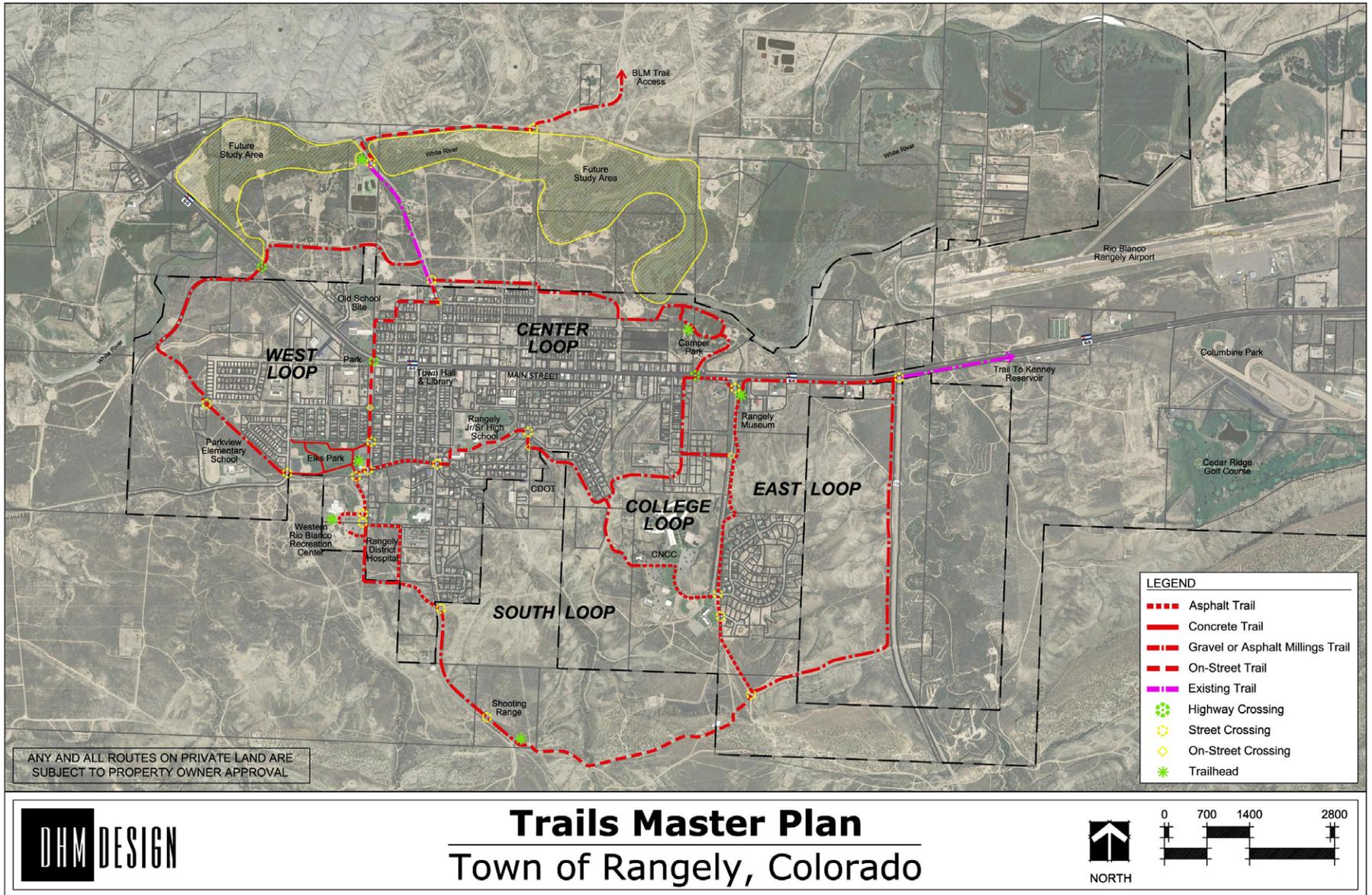
Annual operations and maintenance costs are estimated to run between \$2,500 and \$8,000 per mile depending on the level of improvement and maintenance. Maintenance of the on-road/backroads network would be next to nil assuming the road agencies continue to maintain the roads for vehicular use. Volunteers may be able to adopt sections of the trail and provide some support maintenance such as litter pick up and upkeep of wayfinding signs and other improvements not requiring heavy equipment or special skills. The partners may choose to each maintain their respective segments or may plan to cooperatively pool maintenance resources and management through working agreements.

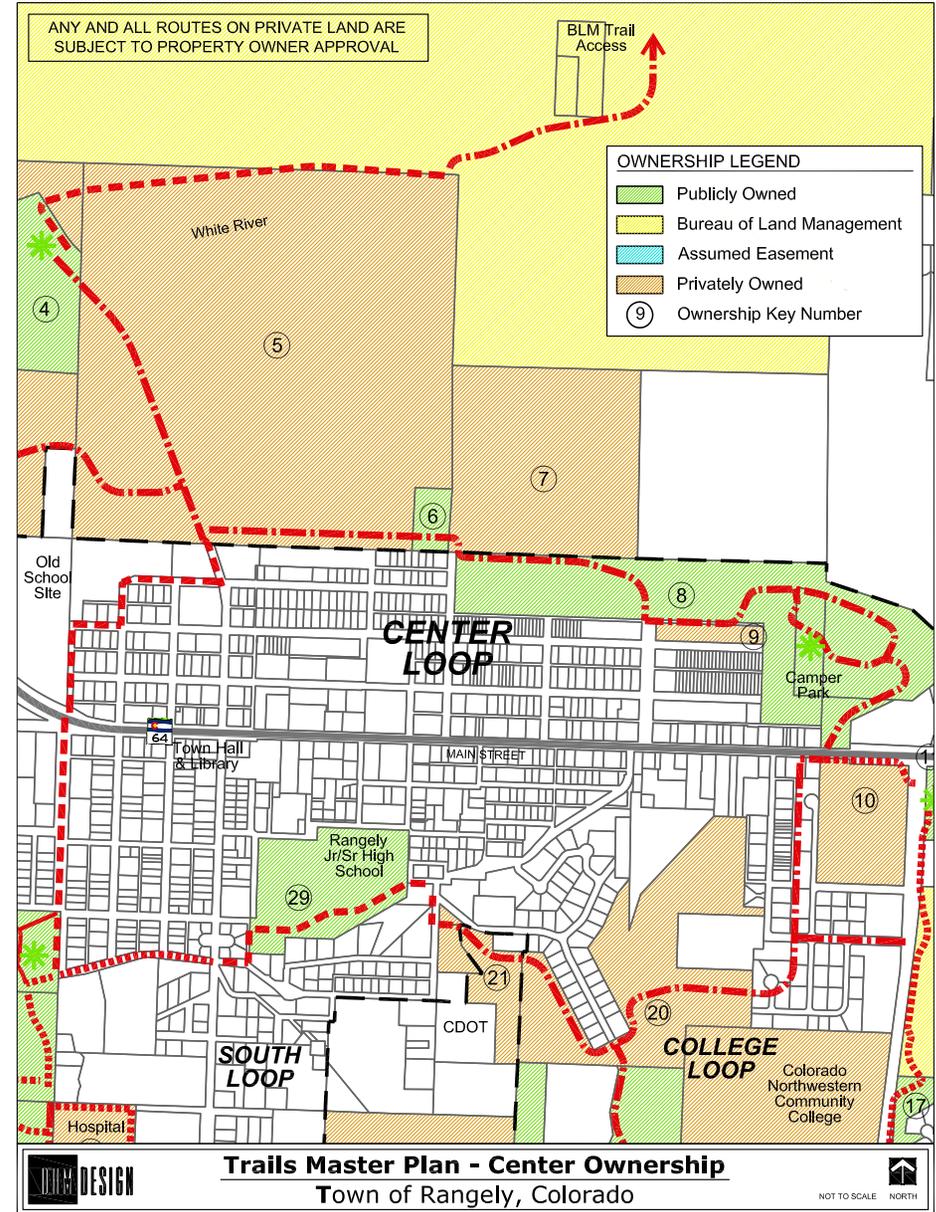
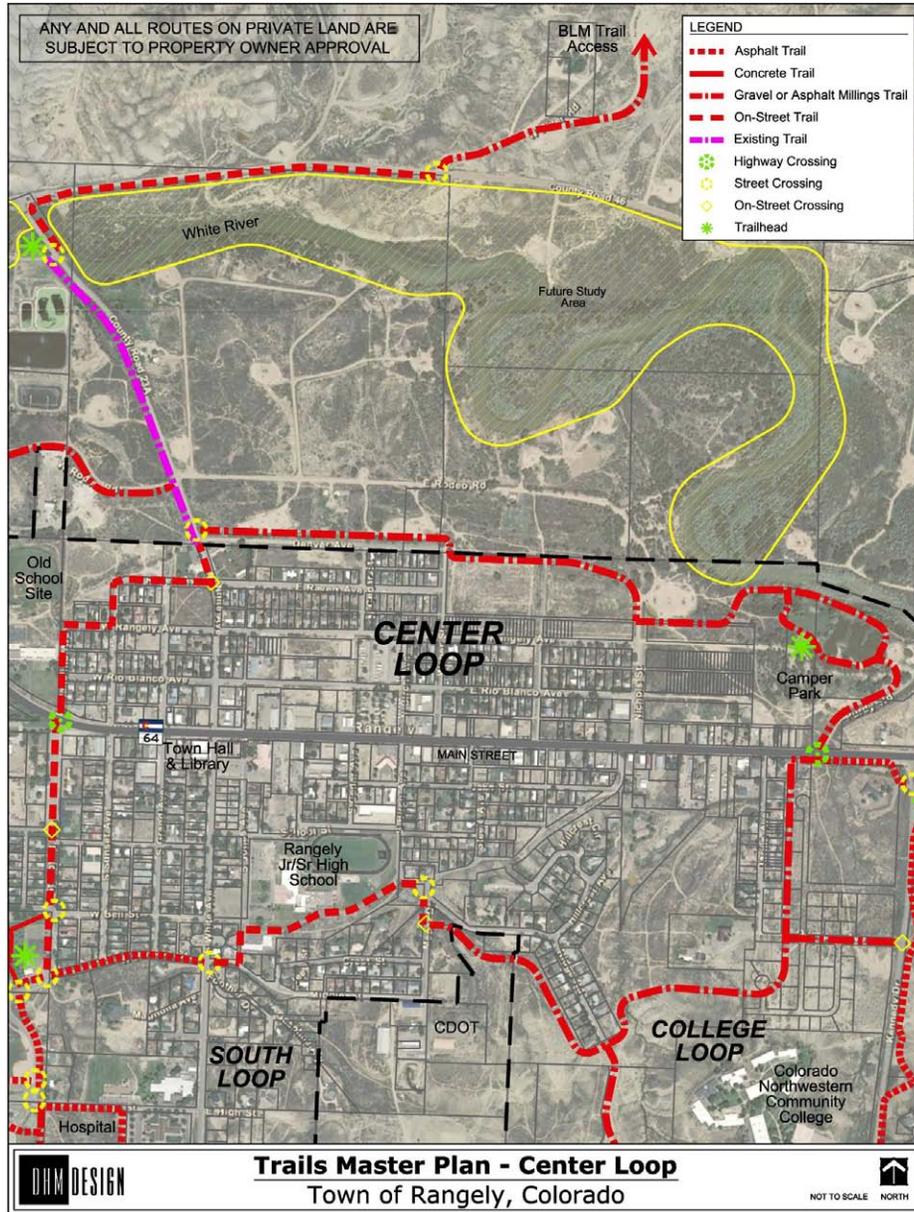


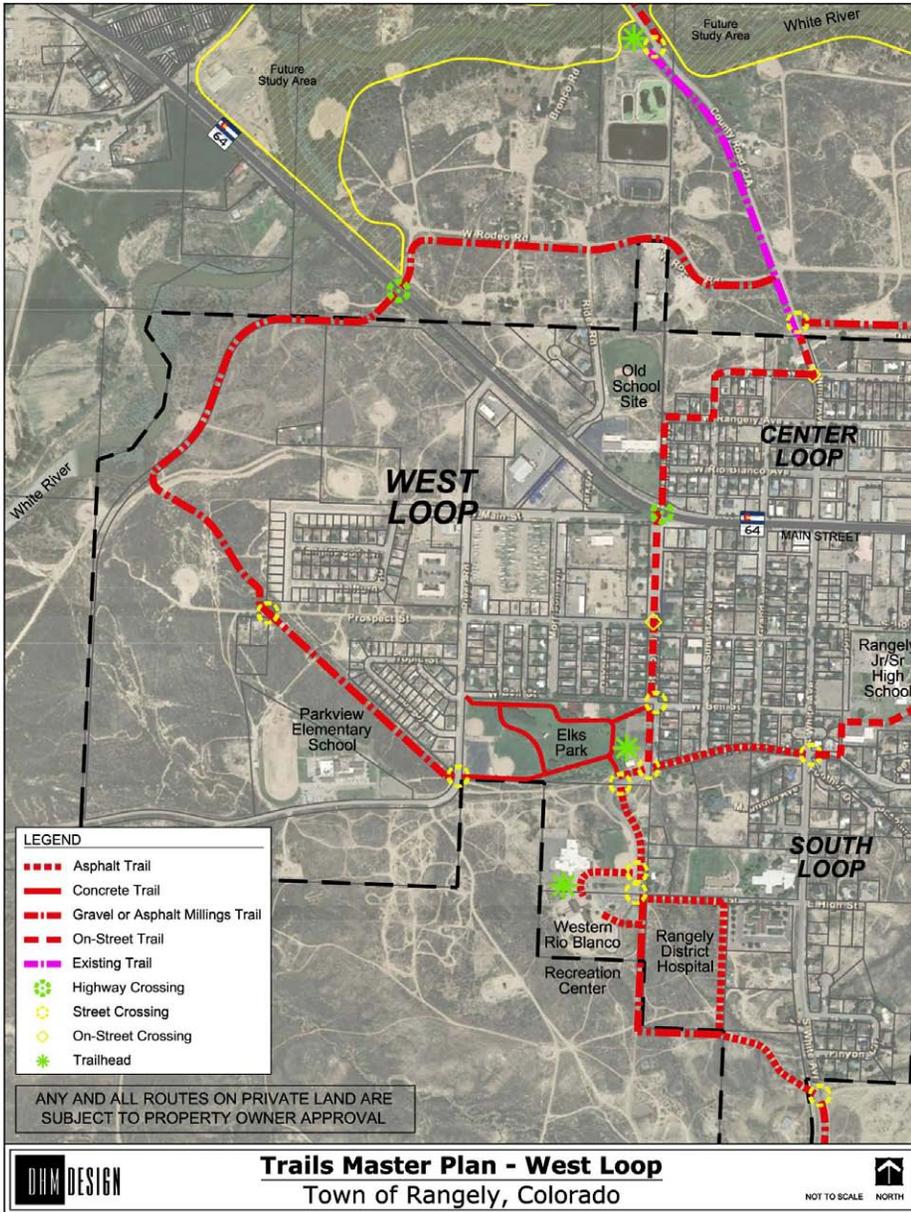
# APPENDIX

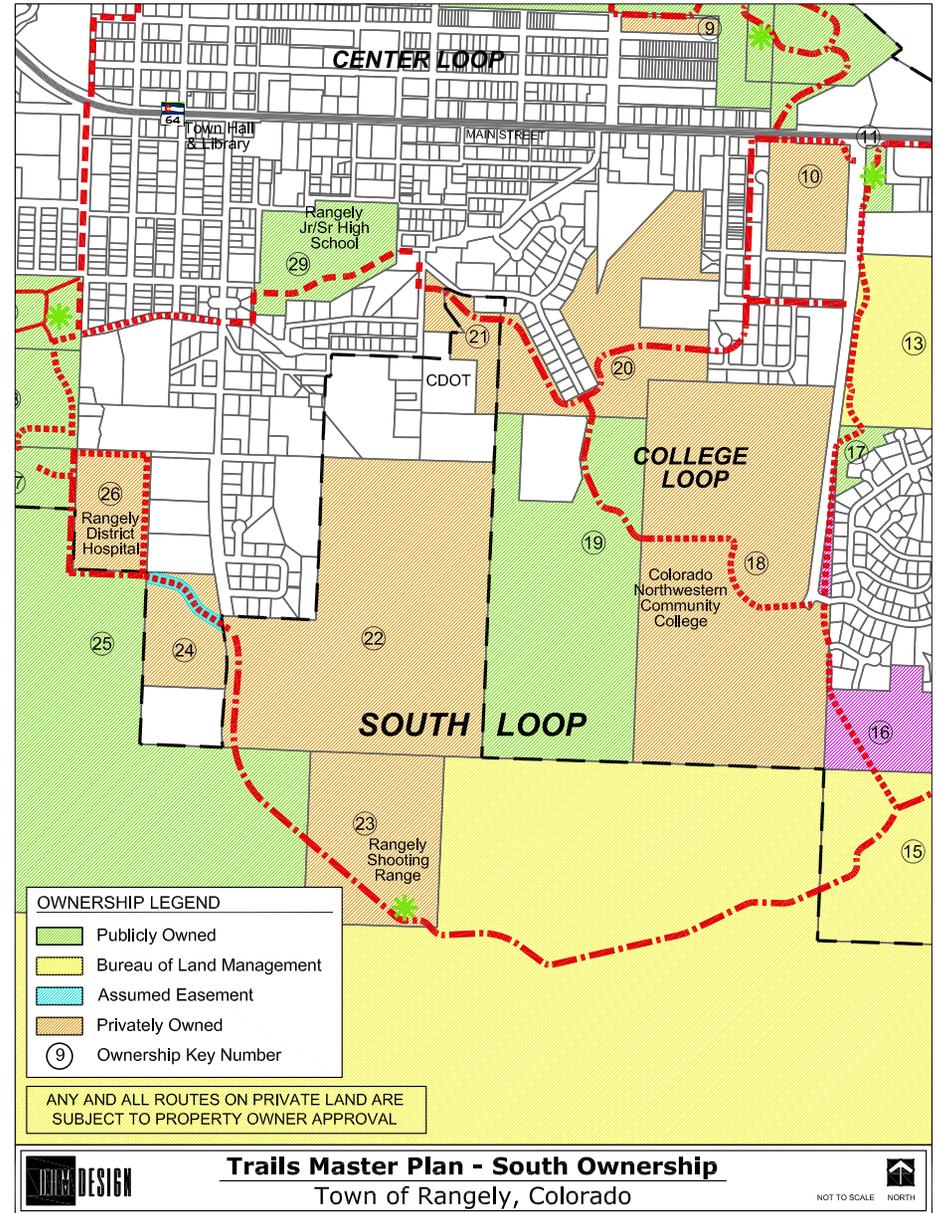
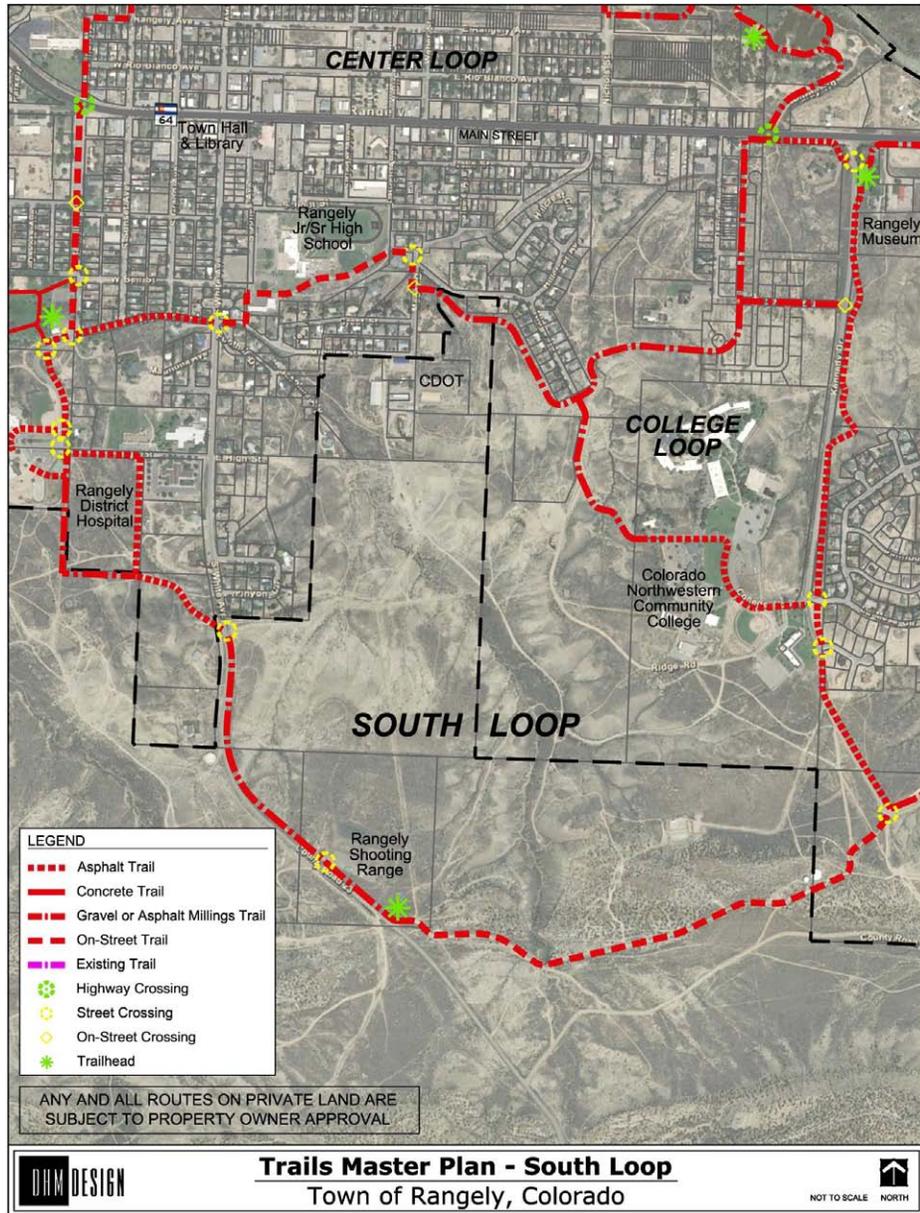


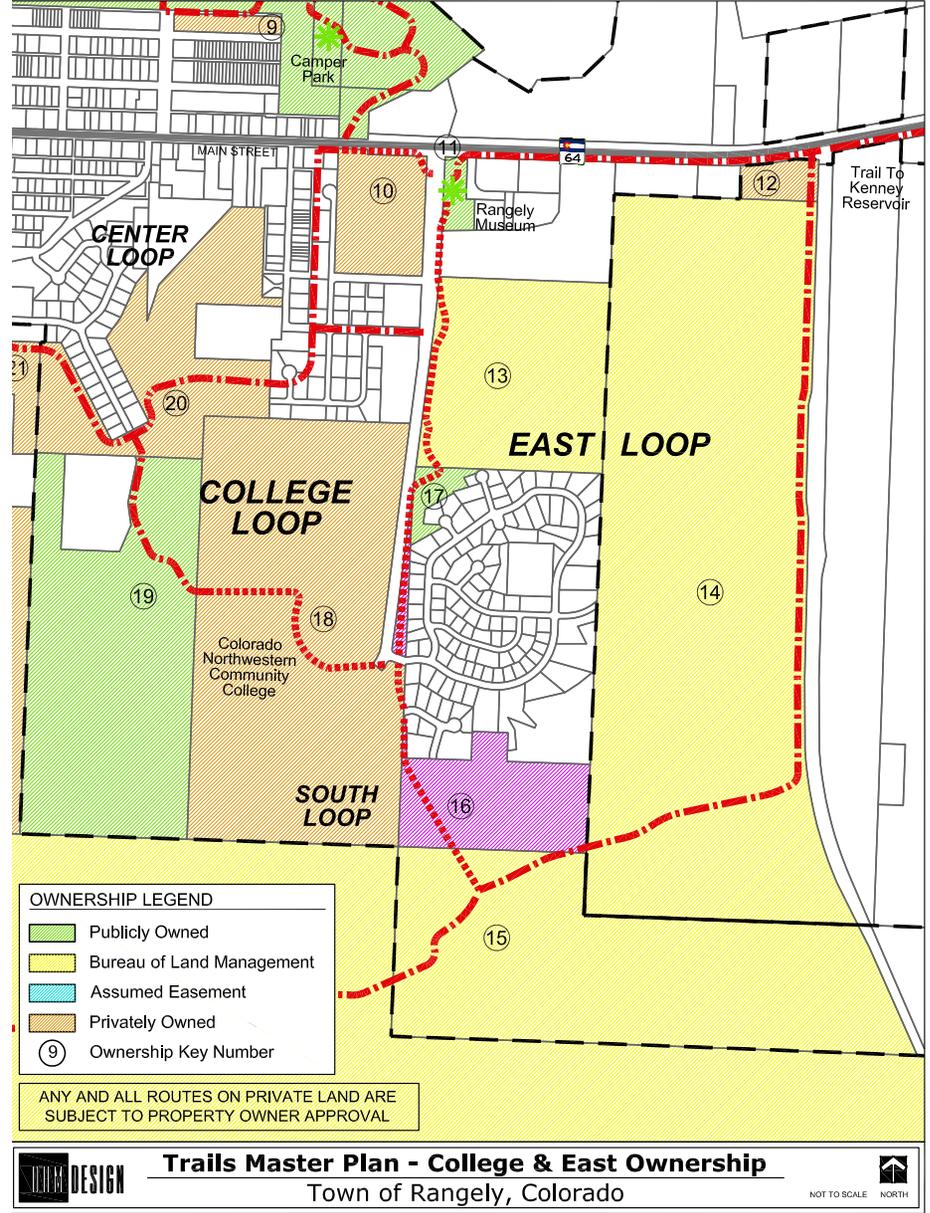
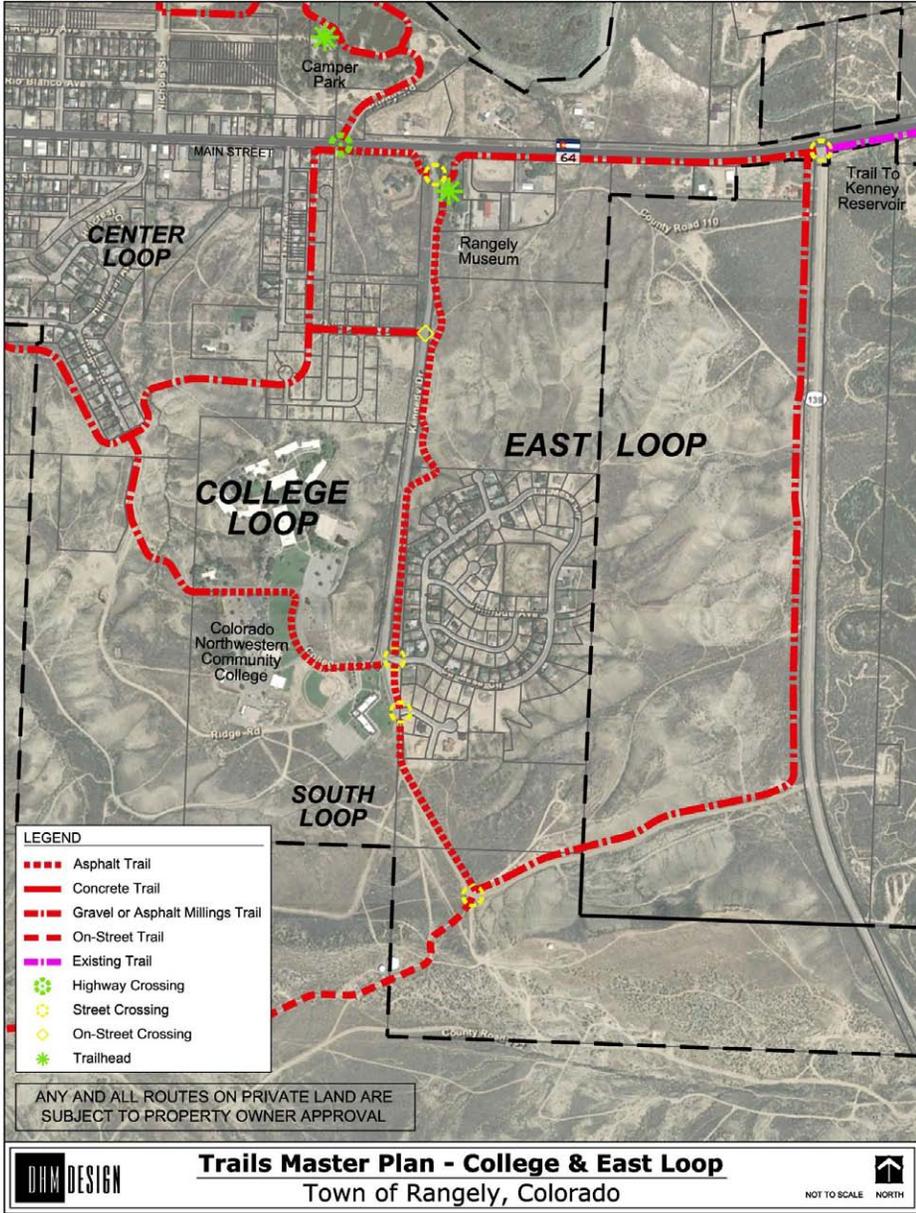
- A. Plan Layout and Cross Sections
- B. Ownership Maps
- C. Preliminary Plans



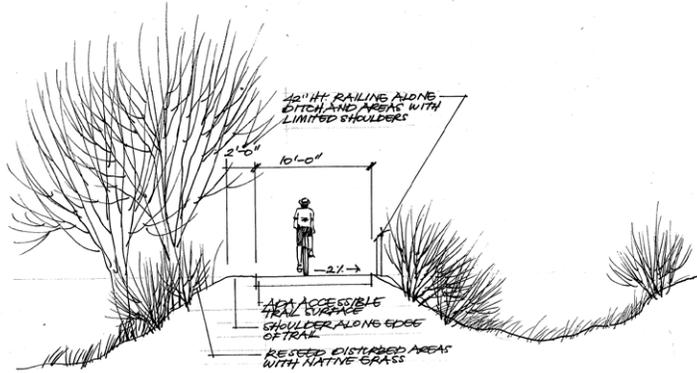




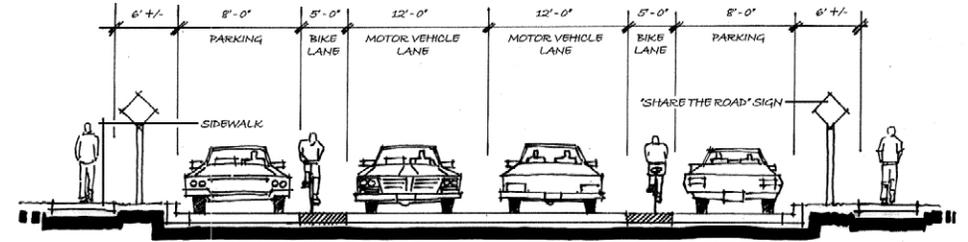




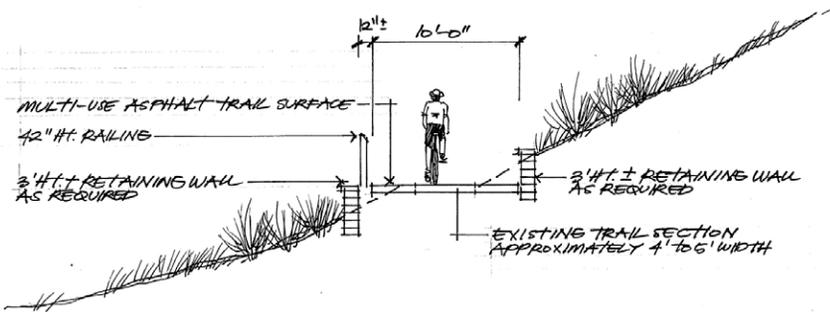
	A	B	C	D	E	F	G	H
1	139302300929	RANGELY KEVIN POOLE MEMORIAL PARK	WESTERN RIO BLANCO METRO PARK & REC		RANGELY	CO	81648	
2	139303401002	FOUNDATION ENERGY FUND III-B HOLDIN, LLC		14800 LANDMARK BLVD., SUITE 220	DALLAS	TX	75254	
3	138335300332	COLTHARP, RICHARD N.		8 QUAIL LANE	BOISE	ID	83716	
4	138335400930	RANGELY SANITATION PLANT			RANGELY	CO	81648	
5	138335400335	CAYWOOD, HOWARD E.		500 HILLARD STREET	TAFT	CA	93268	
6	139301200174	TOWN OF RANGELY		209 EAST MAIN STREET	RANGELY	CO	81648	
7	138335300332	COLTHARP, RICHARD N.		8 QUAIL LANE	BOISE	ID	83716	
8	139301100167	RANGELY, TOWN OF		209 EAST MAIN STREET	RANGELY	CO	81648	
9	139301207001	COLTHARP, FAWN B. TRUSTEE OR SUCCESSORS	COLTHARP, FAWN B. FAMILY LIVING TRUST	2142 EASTWOOD BLVD	OGDEN	UT	84403-5359	
10	139301101006	WALLER, JOHN PATRICK		P O BOX 3313	GLENWOOD	CO	81602	
11	139301100404	RANGELY MUSEUM			RANGELY	CO	81648	
12	139506200023	HAWKINS, JOHN TAYLOR & AMORETTE L.		1349 EAST MAIN STREET	RANGELY	CO	81648	
13	113536100002	BUREAU OF LAND MANAGEMENT	220 MARKET STREET		MEEKER,	CO	81641	
14	113536100002	BUREAU OF LAND MANAGEMENT	220 MARKET STREET		MEEKER,	CO	81641	
15	113536100002	BUREAU OF LAND MANAGEMENT	220 MARKET STREET		MEEKER,	CO	81641	
16	139310100996							
17	1393000RAOPN	RANGELY STREET RIGHT AWAYS						
18	139301400185	COLORADO NORTHWESTERN COMMUNITY COLLEGE						
19	139301300186	RANGELY PUBLIC SCHOOLS						
20	139301206023	HEINLE, JEFF & SHARON		277 CREST STREET	RANGELY	CO		
21	139301300102	GREEN, FRED F. & FRANCES H.		P O BOX 693	RANGELY	CO	81648	
22	139312200004	HEINLE, JEFF & SHARON		277 CREST STREET	RANGELY	CO		
23	139312200166	FRATERNAL ORDER OF THE ELKS						
24	139311100164	WARD, TODD & SARAH						
25	139310100996	CO STATE LAND BOARD		P.O. Box 1094	CRAIG	CO		
26	139302400073	RANGELY DISTRICT HOSPITAL						
27	139302400073	WESTERN RIO BLANCO RECREATION CENTER						
28	139302400072	RANGELY PUBLIC SCHOOLS						
29	139301207062	RANGELY HIGH SCHOOL			RANGELY	CO	81648	
30								



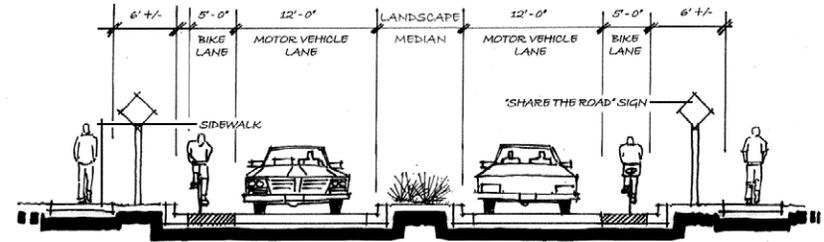
Section 1 - Royden Ditch



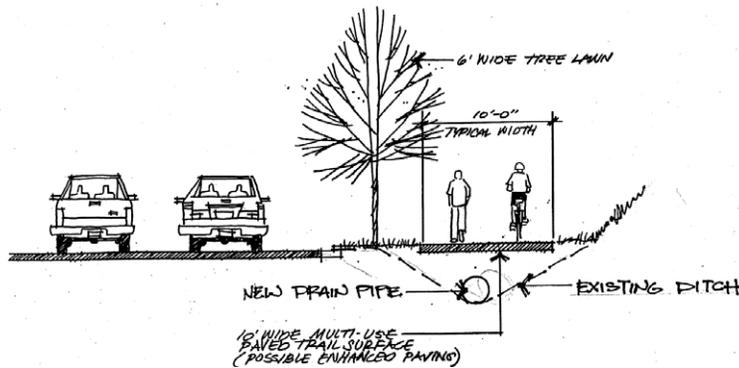
Section 2 - Stanolind Ave South of Highway 64



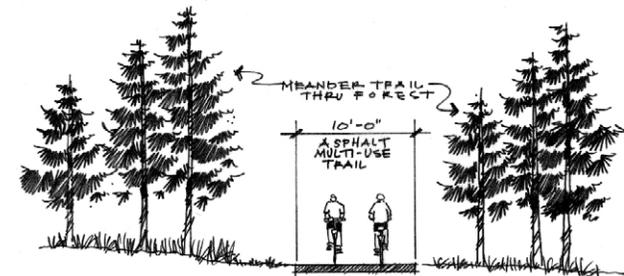
Section 3 - Kennedy Drive with Retaining Walls



Section 4 - Stanolind Ave North of Elks Park



Section 5 - Kennedy Drive



Section 6 - Typical Paved Trail Section



### Section 7 - Shared Use of Asphalt Road Conditions



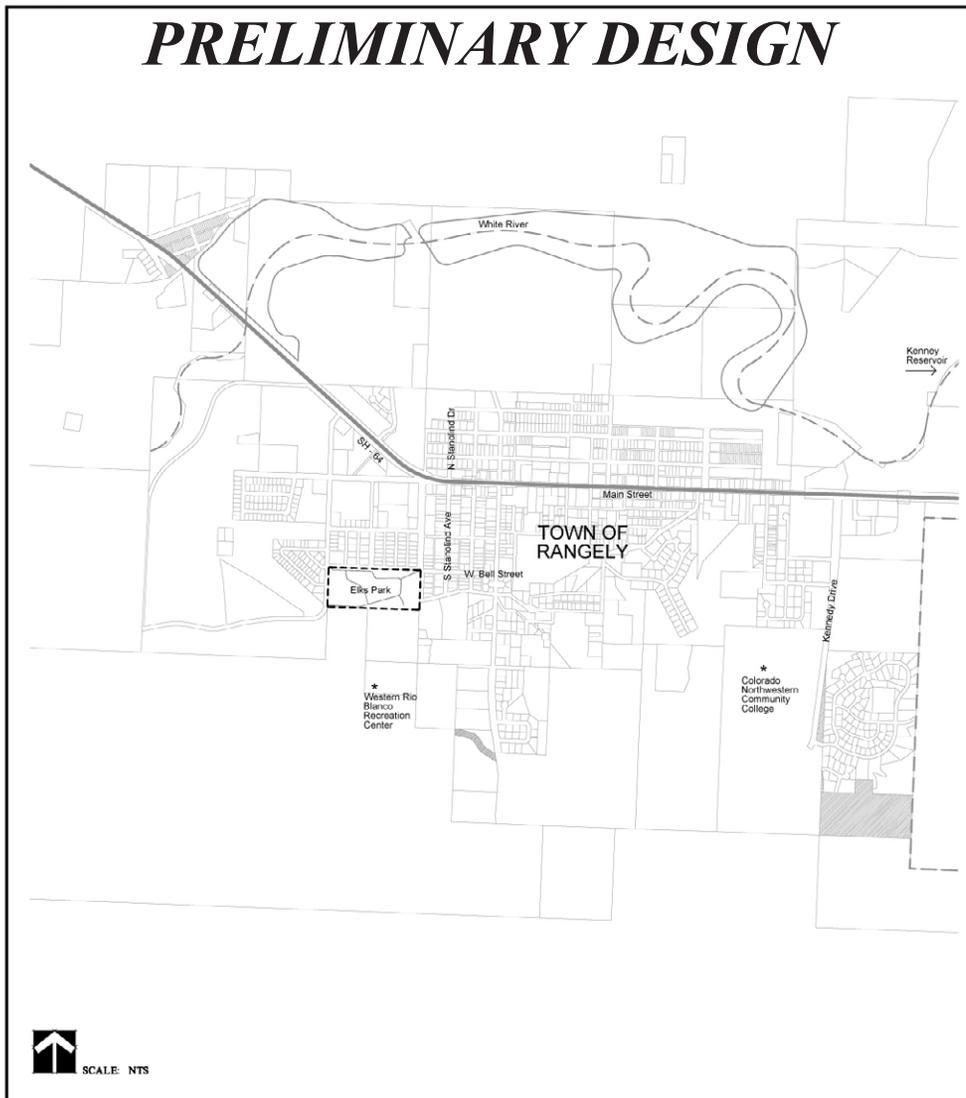
# Rangely Trail Masterplan

## Elks Park Loop Trail Town of Rangely, Colorado

OVERALL MAP



VICINITY PLAN



SHEET INDEX

CV1	COVER SHEET
N1	GENERAL NOTES & CONDITIONS
L1	LAYOUT AND MATERIALS PLAN
SD1	SITE DETAILS

CONSULTANTS:

LANDSCAPE ARCHITECT:



LANDSCAPE ARCHITECTURE  
LAND PLANNING  
URBAN DESIGN

1910 Lawrence Street, Suite 100  
Denver, CO 80204 | 303.892.5564

DRAWING INFORMATION

PREPARED FOR:	TOWN OF RANGELY
PREPARED BY:	DHM DESIGN CORPORATION
PROJECT NUMBER:	00
DRAWN BY:	AN
CHECKED BY:	MW
SUBMITTAL DATE:	AUGUST 12, 2011
REVISIONS:	

SHEET TITLE:

COVER SHEET
JOB DESCRIPTION:
DESIGN DEVELOPMENT
SHEET NUMBER:

CV1

OVERALL GENERAL GRADING NOTES

1. THE CONTRACTOR SHALL OBTAIN AT HIS EXPENSE ALL PERMITS WHICH ARE NECESSARY TO PERFORM THE PROPOSED WORK.
2. THE CONTRACTOR SHALL HAVE ON HIS POSSESSION AT ALL TIMES AND AT THE SITE A COPY OF THE APPROVED CONSTRUCTION PLANS.

BIKEPATH GENERAL NOTES

1. EARTHWORK OPERATIONS SHALL BE IN ACCORDANCE WITH RECOMMENDATIONS MADE IN THE GEOTECHNICAL REPORT FOR THE PROJECT.
2. PAVING SHALL NOT START UNTIL SUBGRADE COMPACTING TESTS ARE TAKEN AND MEET THE REQUIREMENTS OF THE PLANS AND SPECS AND FINAL PAVEMENT DESIGN FOR THE PROJECT THE MINIMUM DEPTH OF ASPHALT SHALL BE 3 INCHES ON THE PATH OR 4 ROADBASE UNLESS A THICKER DESIGN IS RECOMMENDED BY THE GEOTECHNICAL ENGINEER
3. THE CONTRACTOR SHALL SAW CUT ALL EXISTING PAVEMENT WHERE MATCH LINES WITH EXISTING EDGE OF PAVEMENT OCCUR.
4. PATHWAY RETAINING WALL VERTICAL AND HORIZONTAL INFORMATION WILL NEED TO BE ESTABLISHED AS PART OF THESE RKE PATH PLANS. STRUCTURAL, GEOTECHNICAL, AND DRAINAGE ENGINEERING FOR THE WALLS SHALL BE PROVIDED BY THE CONTRACTOR
5. DURING EARTHWORK OPERATION GEOTECHNICAL ENGINEER SHALL ASSESS ACTUAL SUB-SURFACE CONDITIONS AND MAY REQUEST DESIGN MODIFICATIONS IF NECESSARY AND IF APPROVED BY THE TOWN.
6. ALL UTILITIES ON THE DRAWINGS REFLECT APPROXIMATE LOCATIONS. THE CONTRACTOR IS TO VERIFY EXACT LOCATIONS OF EXISTING UTILITIES PRIOR TO BEGINNING CONSTRUCTION OPERATIONS
7. CONTRACTOR SHALL CONSULT WITH ALL PUBLIC UTILITIES RELATIVE TO THE POSSIBILITY OF UNKNOWN UTILITY SERVICES PASSING THROUGH THE SITE. ALL SUCH LINES, STRUCTURES AND SERVICES SHALL BE PROTECTED FROM DAMAGE. IF DAMAGE SHOULD OCCUR DURING CONSTRUCTION ACTIVITIES, THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL REPAIR ALL DAMAGES AS REQUIRED BY THE UTILITY COMPANY AFFECTED.
8. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FAMILIARIZE HIMSELF WITH THE SITE AND TO VERIFY THE QUANTITIES AND LOCATION OF ITEMS TO BE CLEANED UP AND REMOVED.
9. ALL USABLE SALVAGED MATERIALS SHALL BE TURNED OVER TO THE TOWN IF REQUESTED BY THE TOWN.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR OF ANY DAMAGE TO EXISTING UTILITIES AND/OR OTHER EXISTING INFRASTRUCTURE OR FEATURES THAT MAY OCCUR DURING THE COURSE OF WORK. ALL SUCH COSTS SHALL BE THE CONTRACTORS RESPONSIBILITY.
11. THE CONTRACTOR SHALL PROTECT ALL VEGETATION TO REMAIN DURING CONSTRUCTION WITH TEMPORARY 4 FT. PLASTIC ORANGE BARRIER FENCE WITH METAL T-POST OR VIA OTHER APPROVED ALTERNATIVE.
12. TEMPORARY STAGING AREAS SHALL BE THOSE AREAS IDENTIFIED ON THE PLANS UNLESS A PROPOSED ALTERNATIVE IS APPROVED BY THE TOWN OF RANGELY. ALL ACCESS, CONSTRUCTION AND STAGING SHALL OCCUR WITHIN TOWN OWNED PROPERTY OR EASEMENTS UNLESS PRIVATE PROPERTY OWNER APPROVAL IS GRANTED.
13. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL MEET WITH THE TOWN TO IDENTIFY AND MARK TREES THAT ARE TO BE REMOVED, PROTECTED OR SAVED.
14. IT WILL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY QUANTITIES OF TREES TO BE REMOVED & INCLUDE WITHIN THEIR CONSTRUCTION COST. ALL STIMMS ARE TO BE REMOVED OR GROUND TO BELOW GRADE.
15. CONTRACTOR TO PRUNE BRANCHES OFF OF EXISTING TREES THAT CONFLICT WITH TRAIL USERS. A 10' WIDE X 10' HIGH BOX ALONG THE TRAIL SHALL BE KEPT FREE FROM ALL BRANCHES & OTHER OBSTRUCTIONS. CONTRACTOR TO VERIFY SIZE & QUANTITY AND INCLUDE WITHIN THEIR COST OF CONSTRUCTION.
16. A PRE-CONSTRUCTION MEETING SHALL BE HELD WITH THE CONTRACTOR AND TOWN PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITIES.

EROSION CONTROL NOTES

1. THE CONTRACTOR MUST NOTIFY TOWN OF RANGELY AT LEAST 48 HOURS PRIOR TO STARTING CONSTRUCTION. EROSION CONTROL MEASURES MUST BE INSTALLED PRIOR TO GRADING ACTIVITIES.
2. ALL EROSION AND SEDIMENT CONTROL PRACTICES MUST BE MAINTAINED AND REPAIRED AND/OR REPLACED AS NEEDED TO ASSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION.
3. THE CONTRACTOR IS RESPONSIBLE FOR CLEAN UP AND REMOVAL OF ALL DEBRIS AND SHALL MAINTAIN A CLEAN SAFE CONSTRUCTION SITE.
4. FOR DUST PALLIATIVE WILL NOT BE PAID FOR SEPARATELY, RESULTING FROM GRADING ACTIVITIES AND/OR WIND SHALL BE CONTROLLED WITH WATER. CONTRACTOR SHALL BE RESPONSIBLE FOR THIS AT NO EXTRA COST TO THE CONTRACT.

GENERAL NOTES

1. WATER SHALL BE USED AS A DUST PALLIATIVE AS NECESSARY. LOCATIONS SHALL BE AS ORDERED. WATER FOR DUST PALLIATIVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF WORK.
2. EXCAVATION REQUIRED FOR COMPACTION OF BASES OF CUTS AND FILLS WILL BE CONSIDERED AS SURSIDIARY TO THAT OPERATION AND WILL NOT BE PAID FOR SEPARATELY.
3. TYPE OF COMPACTION FOR THIS PROJECT WILL BE AASHTO 199.
4. THE CONTRACTOR SHALL PROTECT ALL EXISTING SURVEY MONUMENTATION DESIGNED TO REMAIN FROM DAMAGE DURING CONSTRUCTION OPERATIONS. ANY MONUMENTS DISTURBED BY THE CONTRACTOR SHALL BE RESET AT HIS OWN EXPENSE. THE CONTRACTOR AND ENGINEER SHALL NOTE THOSE MONUMENTS IN THE FIELD PRIOR TO CONSTRUCTION.
5. UTILITY LINES AS SHOWN ON THE PLAN SHEETS ARE PLOTTED FROM THE BEST AVAILABLE INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD LOCATING AND VERIFYING ALL UTILITY INFORMATION. FOR UTILITY CONSTRUCTION COORDINATION, THE CONTRACTOR WILL CALL THE UTILITY NOTIFICATION CENTER OF COLORADO AT 1-800-922-1987, AT LEAST TWO BUSINESS DAYS PRIOR TO EXCAVATION (NOT INCLUDING THE DAY OF THE CALL).
6. CONTRACTOR SHALL LIMIT CONSTRUCTION ACTIVITIES TO THOSE AREAS OWNED BY THE TOWN OF RANGELY OR WITHIN THE EXISTING EASEMENTS. ANY DISTURBANCES BEYOND THESE LIMITS SHALL BE RESTORED TO ORIGINAL CONDITION BY THE CONTRACTOR AT THE CONTRACTORS EXPENSE. CONSTRUCTION ACTIVITIES IN ADDITION TO NORMAL CONSTRUCTION PROCEDURE SHALL INCLUDE THE PARKING OF VEHICLES OR EQUIPMENT, DISPOSAL OF LITTER AND ANY OTHER ACTION WHICH WOULD ALTER EXISTING CONDITIONS.
7. TOPSOIL MAY BE GENERATED WITHIN PROJECT LIMITS. IF TOPSOIL IS NOT GENERATED WITHIN THE PROJECT LIMITS, IT SHALL BE INSPECTED AT ITS ORIGINAL LOCATION TO ENSURE THERE ARE NO WEEDS. ONCE TOPSOIL HAS BEEN PLACED, PERIODIC INSPECTIONS WILL TAKE PLACE TO LOOK FOR APPEARANCE OF WEEDS. ANY WEEDS THAT APPEAR WILL BE SPOT SPRAYED ACCORDINGLY. HERBICIDE TREATMENT SHALL BE BY A LICENSED HERBICIDE APPLICATOR AND SHALL BE INCLUDED IN THE COST OF SEEDING (NATIVE). PRE-EMERGENT HERBICIDE TO BE TREFLAN AS MANUFACTURED BY ELANCO COMPANY OR AN APPROVED SUBSTITUTION. HERBICIDES TO BE EPA REGISTERED AND APPROVED, OF TYPE RECOMMENDED BY MANUFACTURER.
8. TEMPORARY FILLS SUCH AS COFFER DAMS AND TEMPORARY ROAD CROSSINGS USING IMPORTED MATERIAL SHALL UTILIZE CLEAN, CHEMICALLY FREE FILL TO AVOID A SUBSTANTIAL INCREASE IN SUSPENDED SOLIDS OR POLLUTION. FILL MATERIAL WILL NOT BE OBTAINED FROM THE LIVE WATER AREA IN THE STREAM UNLESS APPROVED BY DIVISION OF WILDLIFE.

9. EROSION CONTROL SHALL BE ADDRESSED ON ALL PROJECTS THAT INVOLVE WATER RELATED ENVIRONMENTS. TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED AT THE EARLIEST PRACTICABLE TIME CONSISTENT WITH GOOD CONSTRUCTION PRACTICES AND SHALL BE MAINTAINED AS NECESSARY THROUGHOUT THE OPERATION OF THE PROJECT.

10. DURING THE PLANNING AND CONSTRUCTION OF A PROJECT, ALL MEASURES SHALL BE TAKEN TO AVOID DISTURBANCE TO EXISTING VEGETATION. SENSITIVE AREAS SHALL BE FENCED AS NECESSARY.

11. IMMEDIATELY UPON COMPLETION OF A PROJECT, ALL DISTURBED AREAS WILL BE REVEGETATED WITH APPROPRIATE PLANT SPECIES TO PROVIDE BANK STABILIZATION. EROSION CONTROL AND HABITAT REPLACEMENT. TEMPORARY SEEDING WILL BE DONE WHERE NECESSARY.

- "APPROPRIATE" PLANT SPECIES MEANS:
1. SPECIES LIKELY TO BECOME ESTABLISHED ON THE SITE;
  2. SPECIES PRESENT IN THE GENERAL VICINITY, OR CLOSELY RELATED SPECIES;
  3. SPECIES OF VALUE AS WILDLIFE HABITAT.

A JOINT AGENCY SPOT FIELD REVIEW OF SELECTED PROJECTS WILL BE USED TO DETERMINE REVEGETATION SUCCESS AFTER TWO GROWING SEASONS. THIS FIELD REVIEW WILL BE USED TO DETERMINE IF THE QUALITY AND QUANTITY OF ESTABLISHED VEGETATION INDICATES PROBABLE SUCCESS AND IF ADDITIONAL FIELD REVIEWS ARE NECESSARY. WHERE WETLANDS ARE INVOLVED A LONGER PERIOD OF REVIEW MAY BE NEEDED TO DETERMINE SUCCESSFUL GROWTH OF THESE AREAS. IF POSSIBLE, A PHOTOGRAPH OF THE AREA PRIOR TO DISTURBANCE AND DURING THE GROWING SEASON WILL BE PROVIDED BY CDOT FOR COMPARISON. A FINAL JOINT AGENCY FIELD REVIEW MAY BE CONDUCTED AFTER VEGETATION HAS BEEN ESTABLISHED IF DEEMED NECESSARY.

19. ALL SIGNS AND STREET MARKINGS SHALL BE DESIGNED, CONSTRUCTED AND PLACED IN ACCORDANCE WITH THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD), LATEST EDITION, UNLESS OTHERWISE APPROVED BY THE TOWN.

20. MIGRATORY BIRD NESTS MAY BE PRESENT WITHIN THE PROJECT LIMITS DURING THE ACTIVE NESTING SEASON (MARCH 1 TO SEPTEMBER 30). PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL WALK THE PROJECT LIMITS WITH THE LANDSCAPE ARCHITECT TO IDENTIFY OR CONFIRM THAT MIGRATORY BIRDS ARE NOT ACTIVELY NESTING. IMPACTS TO ACTIVE MIGRATORY BIRD NESTS SHALL BE AVOIDED.

21. CONTRACTOR IS RESPONSIBLE FOR ACQUIRING STATE STORMWATER DISCHARGE PERMIT.

SEEDING NOTES

1. EXECUTION
  - A. PROVIDE NATIVE GRASS SEED MIXTURES AS SPECIFIED BY THE TOWN. SEED SHALL NOT EXCEED ONE PERCENT (1%) WEED CONTENT.
  2. SOIL PREPARATION FOR NATIVE SEED AREAS
    - A. NATIVE SEEDING SHALL BE DONE WITH A DRILL EQUIPPED WITH A SATISFACTORY SEEDING MECHANISM, AGITATOR, DOUBLE DISK FURROW OPENERS AND PACKER WHEELS, AND A SEPARATE BOX FOR SMALL SEEDED GRASSES (BRILLION 1/2 IN ONE MECHANICAL SEEDER OR EQUAL). SEED SHALL BE UNIFORMLY SOWN 1/2 AT RIGHT ANGLES TO THE DIRECTION OF THE DIRECTION AND THE OTHER. CONTOUR) SEEDS SHALL BE SOWN AT A DEPTH OF APPROXIMATELY 1/2". ANY AREAS NOT ACCESSIBLE TO 1/4" TO ONE-HALF INCH ( ONE-QUARTER ) OF A DRILL SEEDER SHALL BE HAND BROADCASTED AT DOUBLE THE SEED RATE 1/2" INTO SOIL 1/4" TO AND HAND RAKED.

- B. PROTECT SEEDED AREAS AGAINST EROSION BY SPREADING AND CRIMPING IN HAY.

- C. CERTIFIED (WEED FREE) HAY SHALL BE MECHANICALLY CRIMPED INTO THE SEED BED AT RIGHT ANGLES TO THE CONTOUR TO RESIST SOIL EROSION. APPLY HAY AT A RATE OF 1-1/2 TONS PER ACRE.

- D. ALL DISTURBED AREAS SHALL BE RESEED WITH THE SPECIFIED SEED MIX.

- E. SEED SHALL NOT BE SOWN DURING WINDY WEATHER OR WHEN THE GROUND IS FROZEN OR OTHERWISE UNTILLABLE.

- F. NO FERTILIZER SHALL BE USED ADJACENT TO EXISTING WETLANDS, WETLAND MITIGATION SITES OR STREAMS.

3 SEEDING FOR NATIVE SEED AREAS

- A. SEEDING SHALL BE ACCOMPLISHED BY DRILL SEEDING 0.25" TO 0.5" INTO THE SOIL. IN AREAS INACCESSIBLE TO A DRILL, HAND BROADCAST AT DOUBLE THE RATE & RAKE TO A DEPTH OF 0.25" TO 0.5". HYDRAULIC SEEDING SHOULD BE LIMITED TO SLOPES STEEPER THAN 2:1 OR IF THE SOILS ARE TOO ROCKY FOR DRILLING.

- B. SEED SHALL NOT BE SOWN DURING WINDY WEATHER OR WHEN THE GROUND IS FROZEN OR OTHERWISE UNTILLABLE.

- C. HYDRAULIC SEEDING EQUIPMENT AND ACCESSORIES SHALL CONFORM TO THE EQUIPMENT AND ACCESSORIES DESCRIBED WITHIN TECHNICAL SPECIFICATIONS.

- D. WOOD FIBER HYDRO MULCH WITH TACKIFIER MIXTURE SHALL BE APPLIED TO ALL SEEDED AREAS AT THE RATE OF 2,000 LBS/ACRE FOR WOOD FIBER MULCH AND 100 LBS/ACRE FOR MULCH TACKIFIER.

- E. NO FERTILIZER SHALL BE USED ADJACENT TO EXISTING WETLANDS, WETLAND MITIGATION SITES OR STREAMS.

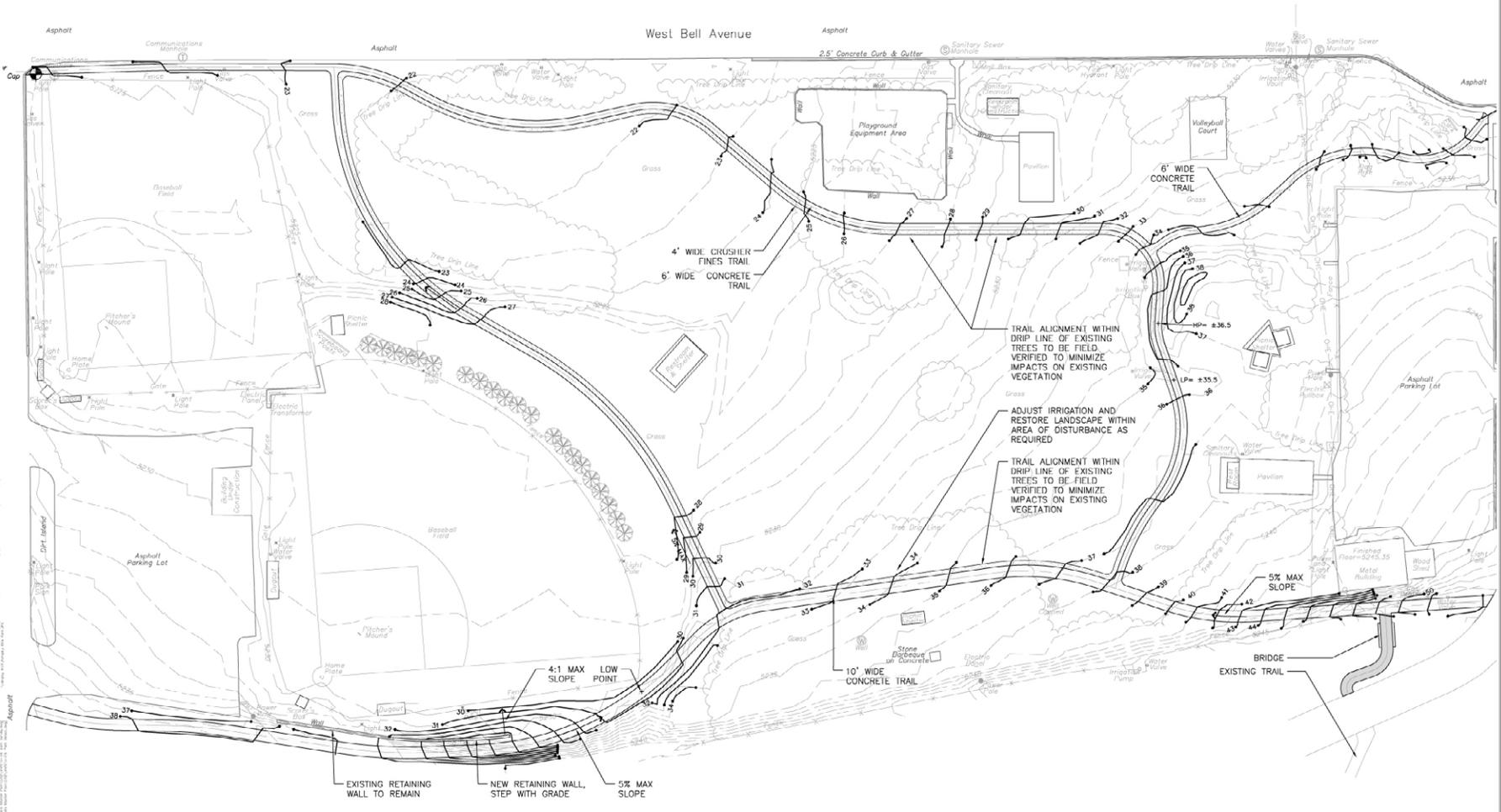
4. ALL DISTURBED AREAS SHALL BE RESEED WITH THE SPECIFIED SEED MIX.

**DDM DESIGN**  
 LANDSCAPE ARCHITECTURE  
 LAND PLANNING  
 URBAN DESIGN  
 1300 Lawrence Street, Suite 100  
 Denver, CO 80204 | 303.972.5566

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Elks Park Trail Improvements  
 Rangely Colorado

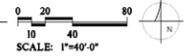
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PROJECT #	DATE
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CHECKED: AN	
DATE: xxxx	
REVISIONS:	
JOB DESCRIPTION:	
SHEET TITLE:	
GENERAL NOTES:	
SHEET NUMBER:	
<b>N1</b>	
SHEET x1 OF x2	



ELKS PARK TRAIL - PLAN

NOTES

1. SURVEY INFORMATION PROVIDED BY HIGH DESERT SURVEYING, LLC. (970) 254-8649
2. RESTORE LANDSCAPE AREAS DISTURBED BY CONSTRUCTION TO EXISTING CONDITIONS
3. PROVIDE EROSION CONTROL BLANKET ON ALL DISTURBED SLOPES GREATER THAN 3:1

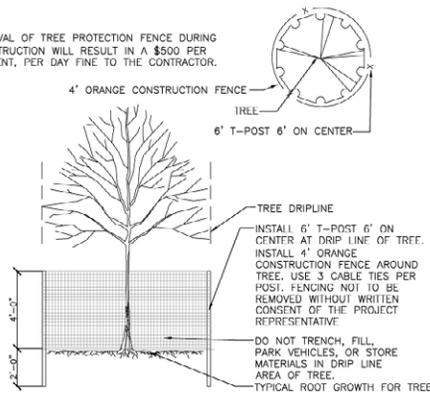


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**ELKS PARK TRAIL IMPROVEMENTS**  
 Rangely Colorado

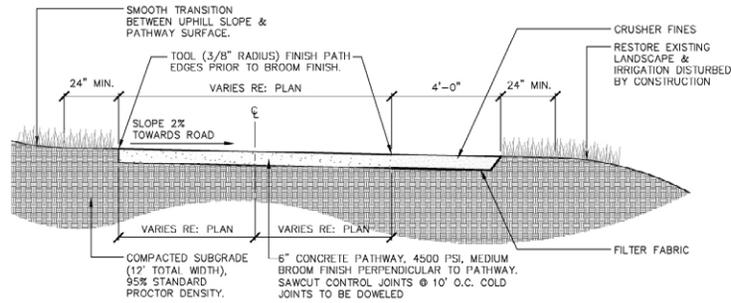
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REVISIONS:	
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LAYOUT PLAN	
SHEET NUMBER:	
<b>L1</b>	
SHEET x1 OF x2	

NOTE:  
REMOVAL OF TREE PROTECTION FENCE DURING CONSTRUCTION WILL RESULT IN A \$500 PER INCIDENT, PER DAY FINE TO THE CONTRACTOR.



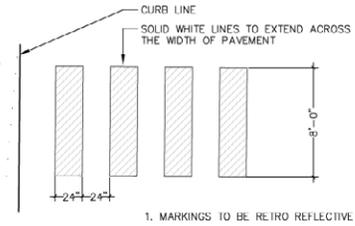
1 TREE PROTECTION

NOT TO SCALE



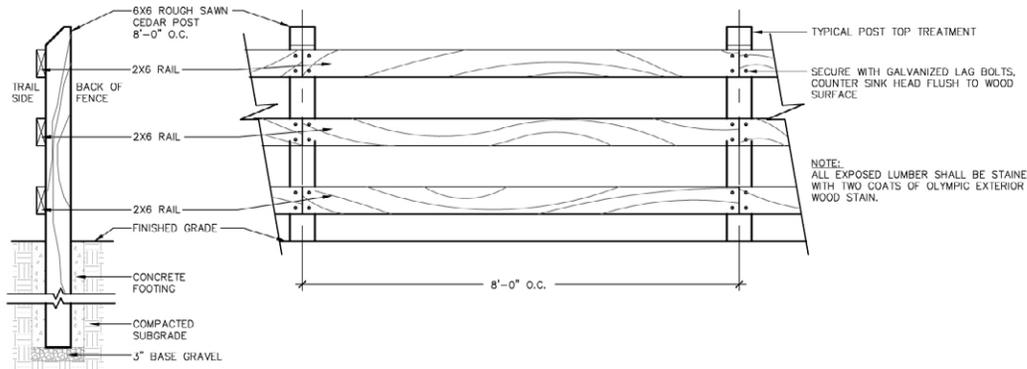
2 CONCRETE / CRUSHER FINE TRAIL

NOT TO SCALE



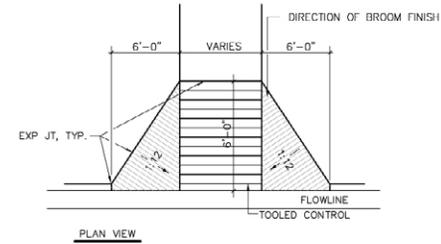
3 PEDESTRIAN CROSSWALK

NOT TO SCALE

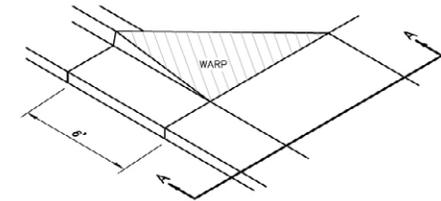


4 3-RAIL WOOD FENCE

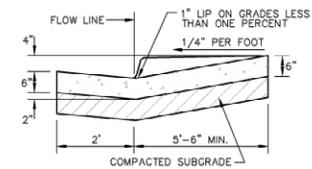
NOT TO SCALE



PLAN VIEW



ISOMETRIC VIEW



SECTION A-A

5 ACCESSIBLE CONCRETE RAMP

NOT TO SCALE



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Elks Park Trail Improvements  
Rangely Colorado

PROJECT NUMBER: \_\_\_\_\_ DATE: \_\_\_\_\_  
Project # \_\_\_\_\_  
DESIGN: MW  
DRAWN: AN  
CHECKED: xxx

REVISIONS:

JOB DESCRIPTION:

SHEET TITLE:

SITE DETAILS:

SHEET NUMBER:

SD1

SHEET x1 OF x2



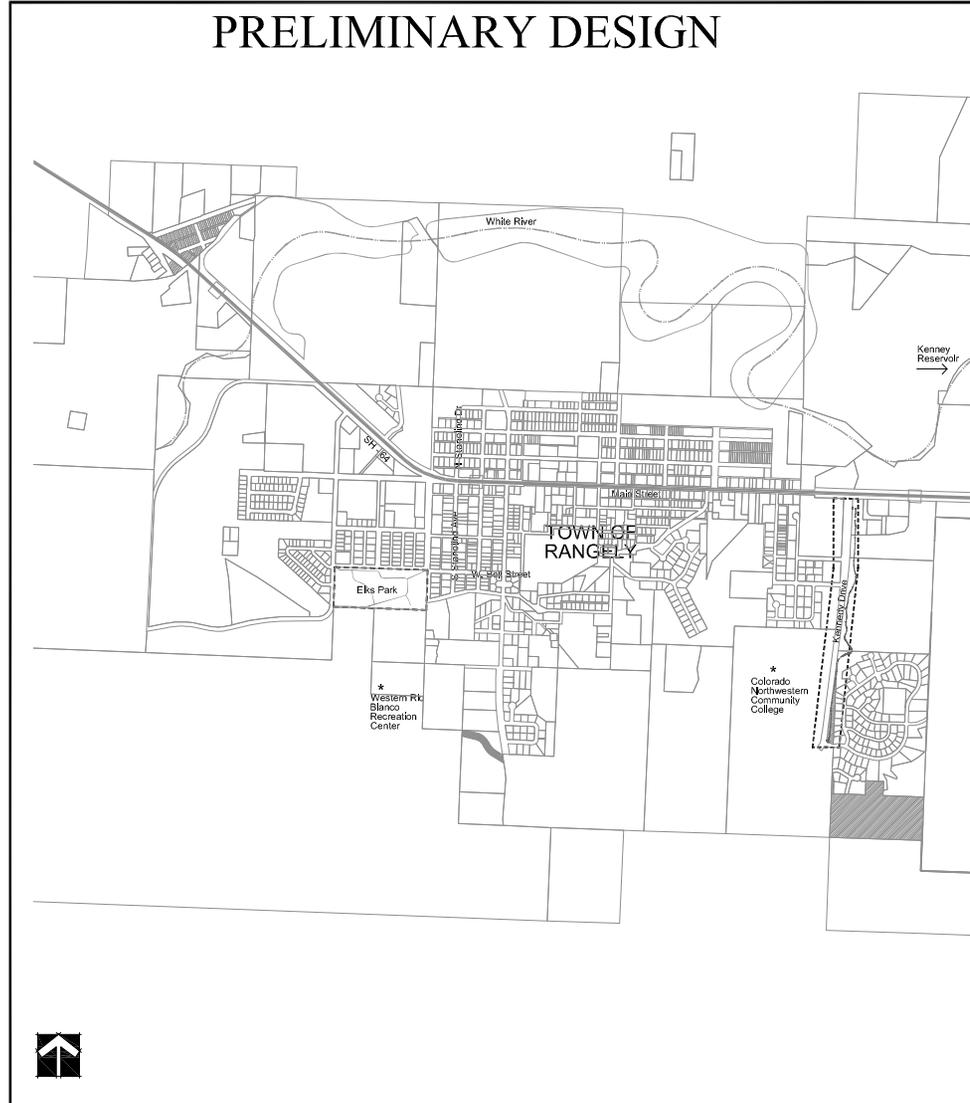
# Rangely Trail Masterplan

## Kennedy Drive Trail Town of Rangely, Colorado

VICINITY MAP



OVERALL PLAN



SHEET INDEX

CV1	COVER SHEET
N1	GENERAL NOTES & CONDITIONS
L1.0-L1.2	LAYOUT AND MATERIALS PLAN
SD1	SITE DETAILS

CONSULTANTS:

LANDSCAPE ARCHITECT:



LANDSCAPE ARCHITECTURE  
LAND PLANNING  
URBAN DESIGN  
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Denver, CO 80202 | 303.672.2584

DRAWING INFORMATION

PREPARED FOR:	TOWN OF RANGELY	SHEET TITLE:	COVER SHEET
PREPARED BY:	DHM DESIGN CORPORATION	JOB DESCRIPTION:	DESIGN DEVELOPMENT
PROJECT NUMBER:	00	SHEET NUMBER:	CV1
DRAWN BY:	AN		
CHECKED BY:	MW		
SUBMITTAL DATE:	AUGUST 25, 2011		
REVISIONS:			

OVERALL GENERAL GRADING NOTES

- 1. THE CONTRACTOR SHALL OBTAIN AT HIS EXPENSE, ALL PERMITS WHICH ARE NECESSARY TO PERFORM THE PROPOSED WORK.
- 2. THE CONTRACTOR SHALL HAVE ON HIS POSSESSION AT ALL TIMES AND AT THE SITE A COPY OF THE APPROVED CONSTRUCTION PLANS.

BIKEPATH GENERAL NOTES

- 1. EARTHWORK OPERATIONS SHALL BE IN ACCORDANCE WITH RECOMMENDATIONS MADE IN THE GEOTECHNICAL REPORT FOR THE PROJECT.
- 2. PAVING SHALL NOT START UNTIL SUBGRADE COMPACTING TESTS ARE TAKEN AND MEET THE REQUIREMENTS OF THE PLANS AND SPECS AND FINAL PAVEMENT DESIGN FOR THE PROJECT THE MINIMUM DEPTH OF ASPHALT SHALL BE 3 INCHES ON THE PATH ON 6" ROADBASE, UNLESS A THICKER DESIGN IS RECOMMENDED BY THE GEOTECHNICAL ENGINEER
- 3. THE CONTRACTOR SHALL SAW-OUT ALL EXISTING PAVEMENT WHERE MATCH LINES WITH EXISTING EDGE OF PAVEMENT OCCUR.
- 4. PATHWAY RETAINING WALL, VERTICAL AND HORIZONTAL INFORMATION WILL NEED TO BE ESTABLISHED AS PART OF THESE BIKE PATH PLANS, STRUCTURAL, GEOTECHNICAL AND DRAINAGE ENGINEERING FOR THE WALLS SHALL BE PROVIDED BY THE CONTRACTOR.
- 5. DURING EARTHWORK OPERATION GEOTECHNICAL ENGINEER SHALL ASSESS ACTUAL SUB-SURFACE CONDITIONS AND MAY REQUEST DESIGN MODIFICATIONS IF NECESSARY AND IF APPROVED BY THE TOWN.
- 6. ALL UTILITIES ON THE DRAWINGS REFLECT APPROXIMATE LOCATIONS. THE CONTRACTOR IS TO VERIFY EXACT LOCATIONS OF EXISTING UTILITIES PRIOR TO BEGINNING CONSTRUCTION OPERATIONS.
- 7. CONTRACTOR SHALL CONSULT WITH ALL PUBLIC UTILITIES RELATIVE TO THE POSSIBILITY OF UNKNOWN UTILITY SERVICES PASSING THROUGH THE SITE. ALL SUCH LINES, STRUCTURES AND SERVICES SHALL BE PROTECTED FROM DAMAGE. IF DAMAGE SHOULD OCCUR DURING CONSTRUCTION ACTIVITIES, THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL REPAIR ALL DAMAGES AS REQUIRED BY THE UTILITY COMPANY AFFECTED.
- 8. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FAMILIARIZE HIMSELF WITH THE SITE AND TO VERIFY THE QUANTITIES AND LOCATION OF ITEMS TO BE CLEANED UP AND REMOVED.
- 9. ALL USABLE SALVAGED MATERIALS SHALL BE TURNED OVER TO THE TOWN, IF REQUESTED BY THE TOWN.
- 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR OF ANY DAMAGE TO EXISTING UTILITIES, AND/OR OTHER EXISTING INFRASTRUCTURE OR FEATURES THAT MAY OCCUR DURING THE COURSE OF WORK. ALL SUCH COSTS SHALL BE THE CONTRACTORS RESPONSIBILITY.
- 11. THE CONTRACTOR SHALL PROTECT ALL VEGETATION TO REMAIN DURING CONSTRUCTION WITH TEMPORARY 4' HT. PLASTIC ORANGE BARRIER FENCE WITH METAL T-POST OR VIA OTHER APPROVED ALTERNATIVE.
- 12. TEMPORARY STAGING AREAS SHALL BE THOSE AREAS IDENTIFIED ON THE PLANS UNLESS A PROPOSED ALTERNATIVE IS APPROVED BY THE TOWN OF RANGELY. ALL ACCESS, CONSTRUCTION AND STAGING SHALL OCCUR WITHIN TOWN OWNED PROPERTY OR EASEMENTS UNLESS PRIVATE PROPERTY OWNER APPROVAL IS GRANTED.
- 13. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL MEET WITH THE TOWN TO IDENTIFY AND MARK TREES THAT ARE TO BE REMOVED, PROTECTED OR SAVED.
- 14. IT WILL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY QUANTITIES OF TREES TO BE REMOVED & INCLUDE WITHIN THEIR CONSTRUCTION COST. ALL STUMPS ARE TO BE REMOVED OR GROUND TO BELOW GRADE
- 15. CONTRACTOR TO PRUNE BRANCHES OFF OF EXISTING TREES THAT CONFLICT WITH TRAIL USERS. A 10' WIDE X 10' HIGH BOX ALONG THE TRAIL SHALL BE KEPT FREE FROM ALL BRANCHES & OTHER OBSTRUCTIONS. THE CONTRACTOR TO VERIFY SIZE & QUANTITY AND INCLUDE WITHIN THEIR COST OF CONSTRUCTION.
- 16. A PRE-CONSTRUCTION MEETING SHALL BE HELD WITH THE CONTRACTOR AND TOWN PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITIES.

EROSION CONTROL NOTES

- 1. THE CONTRACTOR MUST NOTIFY TOWN OF RANGELY AT LEAST 48 HOURS PRIOR TO STARTING CONSTRUCTION. EROSION CONTROL MEASURES MUST BE INSTALLED PRIOR TO GRADING ACTIVITIES.
- 2. ALL EROSION AND SEDIMENT CONTROL PRACTICES MUST BE MAINTAINED AND REPAIRED AND/OR REPLACED AS NEEDED TO ASSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION.
- 3. THE CONTRACTOR IS RESPONSIBLE FOR CLEAN UP AND REMOVAL OF ALL DEBRIS AND SHALL MAINTAIN A CLEAN, SAFE CONSTRUCTION SITE.
- 4. AS NECESSARY AND/OR IF REQUESTED BY THE TOWN, DUST RESULTING FROM GRADING ACTIVITIES AND/OR WIND SHALL BE CONTROLLED WITH WATER. CONTRACTOR SHALL BE RESPONSIBLE FOR THIS AT NO EXTRA COST TO THE CONTRACT.

GENERAL NOTES

- 1. WATER SHALL BE USED AS A DUST PALLIATIVE AS NECESSARY. LOCATIONS SHALL BE AS ORDERED. WATER FOR DUST PALLIATIVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF WORK.
- 2. EXCAVATION REQUIRED FOR COMPACTION OF BASES OF CUTS AND FILLS WILL BE CONSIDERED AS SUBSIDIARY TO THAT OPERATION AND WILL NOT BE PAID FOR SEPARATELY.
- 3. TYPE OF COMPACTION FOR THIS PROJECT WILL BE AASHTO T 99.
- 4. THE CONTRACTOR SHALL PROTECT ALL EXISTING SURVEY MONUMENTATION, DESIGNED TO REMAIN FROM DAMAGE DURING CONSTRUCTION OPERATIONS. ANY MONUMENTS DISTURBED BY THE CONTRACTOR SHALL BE RESET AT HIS OWN EXPENSE. THE CONTRACTOR AND ENGINEER SHALL NOTE THOSE MONUMENTS IN THE FIELD PRIOR TO CONSTRUCTION.
- 5. UTILITY LINES AS SHOWN ON THE PLAN SHEETS ARE PLOTTED FROM THE BEST AVAILABLE INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD LOCATING AND VERIFYING ALL UTILITY INFORMATION FOR UTILITY CONSTRUCTION COORDINATION. THE CONTRACTOR WILL CALL THE UTILITY NOTIFICATION CENTER OF COLORADO AT 1-800-922-1987, AT LEAST TWO BUSINESS DAYS PRIOR TO EXCAVATION (NOT INCLUDING THE DAY OF THE CALL).
- 6. CONTRACTOR SHALL LIMIT CONSTRUCTION ACTIVITIES TO THOSE AREAS OWNED BY THE TOWN OF RANGELY OR WITHIN THE EXISTING EASEMENTS. ANY DISTURBANCES BEYOND THESE LIMITS SHALL BE RESTORED TO ORIGINAL CONDITION BY THE CONTRACTOR AT THE CONTRACTORS EXPENSE. CONSTRUCTION ACTIVITIES IN ADDITION TO NORMAL CONSTRUCTION PROCEDURE SHALL INCLUDE THE PARKING OF VEHICLES OR EQUIPMENT, DISPOSAL OF LITTER AND ANY OTHER ACTION WHICH WOULD ALTER EXISTING CONDITIONS.
- 7. TOPSOIL MAY BE GENERATED WITHIN PROJECT LIMITS. IF TOPSOIL IS NOT GENERATED WITHIN THE PROJECT LIMITS, IT SHALL BE INSPECTED AT ITS ORIGINAL LOCATION TO ENSURE THERE ARE NO WEEDS. ONCE TOPSOIL HAS BEEN PLACED, PERIODIC INSPECTIONS WILL TAKE PLACE TO LOOK FOR APPEARANCE OF WEEDS. ANY WEEDS THAT APPEAR WILL BE SPOT SPRAYED ACCORDINGLY. HERBICIDE TREATMENT SHALL BE BY A LICENSED HERBICIDE APPLICATOR AND SHALL BE INCLUDED IN THE COST OF SEEDING (NATIVE). PRE-EMERGENT HERBICIDE TO BE TRIFLORAL AS MANUFACTURED BY ELANCO COMPANY OR AN APPROVED SUBSTITUTION. HERBICIDES TO BE EPA REGISTERED AND APPROVED, OF TYPE RECOMMENDED BY MANUFACTURER.
- 8. TEMPORARY FILLS, SUCH AS COFFER DAMS AND TEMPORARY ROAD CROSSINGS USING IMPORTED MATERIAL, SHALL UTILIZE CLEAN, CHEMICALLY FREE FILL TO AVOID A SUBSTANTIAL INCREASE IN SUSPENDED SOLIDS OR POLLUTION. FILL MATERIAL WILL NOT BE OBTAINED FROM THE LIVE WATER AREA IN THE STREAM UNLESS APPROVED BY DIVISION OF WILDLIFE.
- 9. EROSION CONTROL SHALL BE ADDRESSED ON ALL PROJECTS THAT INVOLVE WATER RELATED ENVIRONMENTS. TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED AT THE EARLIEST PRACTICABLE TIME CONSISTENT WITH GOOD CONSTRUCTION PRACTICES AND SHALL BE MAINTAINED AS NECESSARY THROUGHOUT THE OPERATION OF THE PROJECT.
- 10. DURING THE PLANNING AND CONSTRUCTION OF A PROJECT, ALL MEASURES SHALL BE TAKEN TO AVOID DISTURBANCE TO EXISTING VEGETATION. SENSITIVE AREAS SHALL BE

FENCED AS NECESSARY.

- 11. IMMEDIATELY UPON COMPLETION OF A PROJECT, ALL DISTURBED AREAS WILL BE REVEGETATED WITH APPROPRIATE PLANT SPECIES TO PROVIDE BANK STABILIZATION, EROSION CONTROL AND HABITAT REPLACEMENT. TEMPORARY SEEDINGS WILL BE DONE WHERE NECESSARY.

"APPROPRIATE" PLANT SPECIES MEANS:  
 1. SPECIES LIKELY TO BECOME ESTABLISHED ON THE SITE;  
 2. SPECIES PRESENT IN THE GENERAL VICINITY, OR CLOSELY RELATED SPECIES;  
 3. SPECIES OF VALUE AS WILDLIFE HABITAT.

A JOINT AGENCY SPOT FIELD REVIEW OF SELECTED PROJECTS WILL BE USED TO DETERMINE REVEGETATION SUCCESS AFTER TWO GROWING SEASONS. THIS FIELD REVIEW WILL BE USED TO DETERMINE IF THE QUALITY AND QUANTITY OF ESTABLISHED VEGETATION INDICATES PROBABLE SUCCESS AND IF ADDITIONAL FIELD REVIEWS ARE NECESSARY. WHERE WETLANDS ARE INVOLVED A LONGER PERIOD OF REVIEW MAY BE NEEDED TO DETERMINE SUCCESSFUL VEGETATION OF THESE AREAS. IF POSSIBLE, A PHOTOGRAPH OF THE AREA PRIOR TO DISTURBANCE AND DURING THE GROWING SEASON WILL BE PROVIDED BY CDOT FOR COMPARISON. A FINAL JOINT AGENCY FIELD REVIEW MAY BE CONDUCTED AFTER VEGETATION HAS BEEN ESTABLISHED IF DEEMED NECESSARY.

- 19. ALL SIGNS AND STREET MARKINGS SHALL BE DESIGNED, CONSTRUCTED AND PLACED IN ACCORDANCES WITH THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) LATEST EDITION, UNLESS OTHERWISE APPROVED BY THE TOWN.

- 20. MIGRATORY BIRD NESTS MAY BE PRESENT WITHIN THE PROJECT LIMITS DURING THE ACTIVE NESTING SEASON (MARCH 1 TO SEPTEMBER 30). PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL WALK THE PROJECT LIMITS WITH THE LANDSCAPE ARCHITECT TO IDENTIFY OR CONFIRM THAT MIGRATORY BIRDS ARE NOT ACTIVELY NESTING. IMPACTS TO ACTIVE MIGRATORY BIRD NESTS SHALL BE AVOIDED.

- 21. CONTRACTOR IS RESPONSIBLE FOR ACQUIRING STATE STORMWATER DISCHARGE PERMIT.

SEEDING NOTES

- 1. EXECUTION
  - A. PROVIDE NATIVE GRASS SEED MIXTURES AS SPECIFIED BY THE TOWN. SEED SHALL NOT EXCEED ONE PERCENT (1%) WEEED CONTENT.
  - 2. SOIL PREPARATION FOR NATIVE SEED AREAS
    - A. NATIVE SEEDING SHALL BE DONE WITH A DRILL EQUIPPED WITH A SATISFACTORY SEEDING MECHANISM, AGITATOR, DOUBLE DISK FURROW OPENERS AND PACKER WHEELS, AND A SEPARATE BOX FOR SMALL SEEDS (BRILLION 1/2 IN ONE MECHANICAL SEEDER OR EQUAL). SEED SHALL BE UNIFORMLY SOWN (1/2 AT RIGHT ANGLES TO THE DIRECTION OF THE DIRECTION AND THE OTHER CONTOUR). SEEDS SHALL BE SOWN AT A DEPTH OF APPROXIMATELY 1/2". ANY AREAS NOT ACCESSIBLE TO 1/4" TO ONE-HALF INCH ( ONE-QUARTER (1/4) A DRILL SEEDER SHALL BE HAND BROADCAST AT DOUBLE THE SEED RATE 1/2" INTO SOIL, 1/4" TO AND HAND RAKED.
    - B. PROTECT SEEDED AREAS AGAINST EROSION BY SPREADING AND CRIMPING IN HAY.
    - C. CERTIFIED (WEED FREE) HAY SHALL BE MECHANICALLY CRIMPED INTO THE SEED BED AT RIGHT ANGLES TO THE CONTOUR TO RESIST SOIL EROSION. APPLY HAY AT A RATE OF 1-1/2 TONS PER ACRE.
    - D. ALL DISTURBED AREAS SHALL BE RESEEDED WITH THE SPECIFIED SEED MIX.
    - E. SEED SHALL NOT BE SOWN DURING WINDY WEATHER OR WHEN THE GROUND IS FROZEN OR OTHERWISE UNTILLABLE.
    - F. NO FERTILIZER SHALL BE USED ADJACENT TO EXISTING WETLANDS, WETLAND MITIGATION SITES OR STREAMS.
- 3. SEEDING FOR NATIVE SEED AREAS
  - A. SEEDING SHALL BE ACCOMPLISHED BY DRILL SEEDING 0.25" TO 0.5" INTO THE SOIL. IN AREAS UNACCESSIBLE TO A DRILL, HAND BROADCAST AT DOUBLE THE RATE & RAKE TO A DEPTH OF 0.25" TO 0.5". HYDRAULIC SEEDING SHOULD BE LIMITED TO SLOPES STEEPER THAN 2:1 OR IF THE SOILS ARE TOO ROCKY FOR DRILLING.
  - B. SEED SHALL NOT BE SOWN DURING WINDY WEATHER OR WHEN THE GROUND IS FROZEN OR OTHERWISE UNTILLABLE.

- C. HYDRAULIC SEEDING EQUIPMENT AND ACCESSORIES SHALL CONFORM TO THE EQUIPMENT AND ACCESSORIES DESCRIBED WITHIN TECHNICAL SPECIFICATIONS.

- D. WOOD FIBER HYDRO MULCH WITH TACKIFIER MIXTURE SHALL BE APPLIED TO ALL SEEDED AREAS AT THE RATE OF 2,000 LBS/ACRE FOR WOOD FIBER MULCH AND 100 LBS/ACRE FOR MULCH TACKIFIER.

- E. NO FERTILIZER SHALL BE USED ADJACENT TO EXISTING WETLANDS, WETLAND MITIGATION SITES OR STREAMS.

- 4. ALL DISTURBED AREAS SHALL BE RESEEDED WITH THE SPECIFIED SEED MIX.

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**Kennedy Drive Trail Improvements**  
 Rangely, Colorado

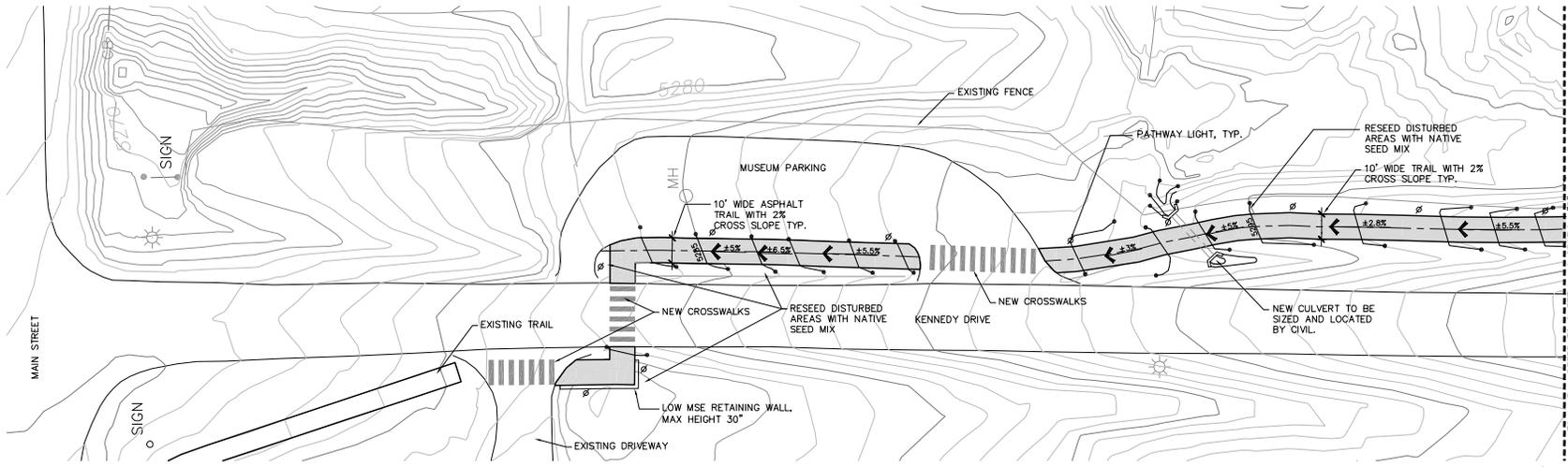
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 DESIGNED: MW  
 DRAWN: AN  
 CHECKED: XXX  
 REVISIONS:

JOB DESCRIPTION:

SHEET TITLE:

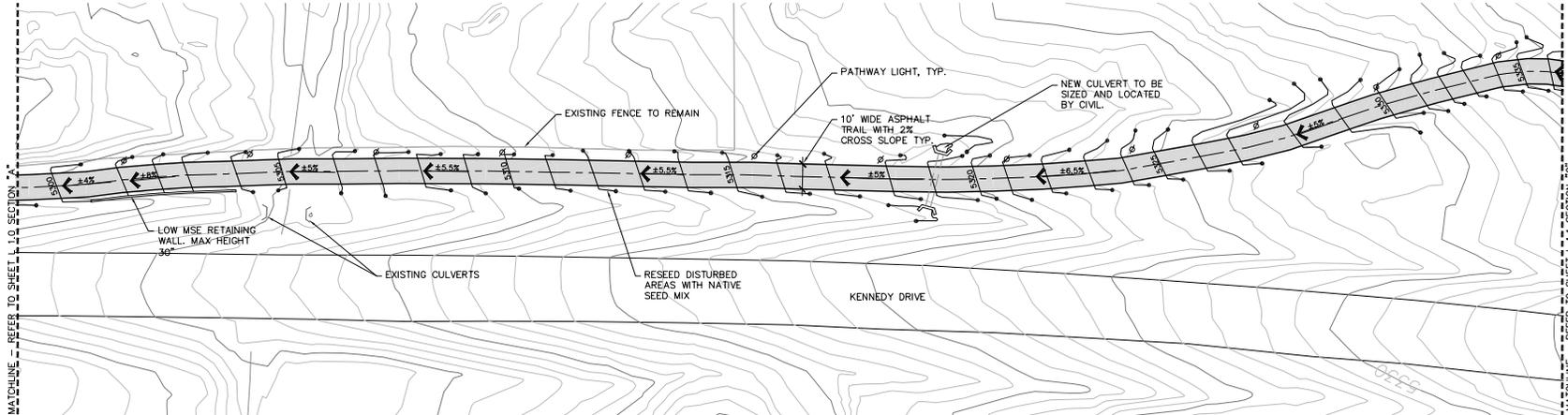
GENERAL NOTES

SHEET NUMBER: N1



KENNEDY DRIVE TRAIL - TRAIL SECTION "A"

SCALE = 1" = 20'-0"



KENNEDY DRIVE TRAIL - TRAIL SECTION "B"

SCALE = 1" = 20'-0"

**NOTES**

1. SURVEY INFORMATION PROVIDED BY JOY SURVEYING COMPANY, (970) 878-5292
2. RESTORE LANDSCAPE AREAS DISTURBED BY CONSTRUCTION TO EXISTING CONDITIONS
3. PROVIDE EROSION CONTROL BLANKET ON ALL DISTURBED SLOPES GREATER THAN 3:1

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Rangely, Colorado

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DRAWN: ANX  
REVISIONS:

JOB DESCRIPTION:

SHEET TITLE: LAYOUT & GRADING PLAN

SHEET NUMBER: L1.0



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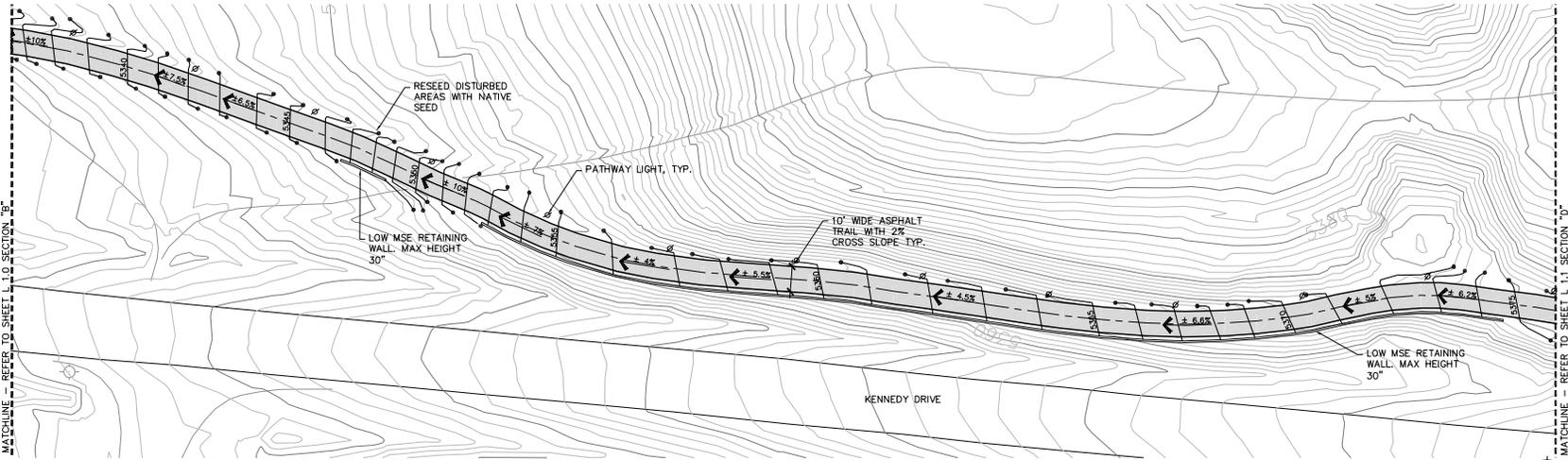
# Kennedy Drive Trail Improvements

Rangely, Colorado

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DESIGNED: MW  
DRAWN: AN  
CHECKED: XXX  
REVISIONS:

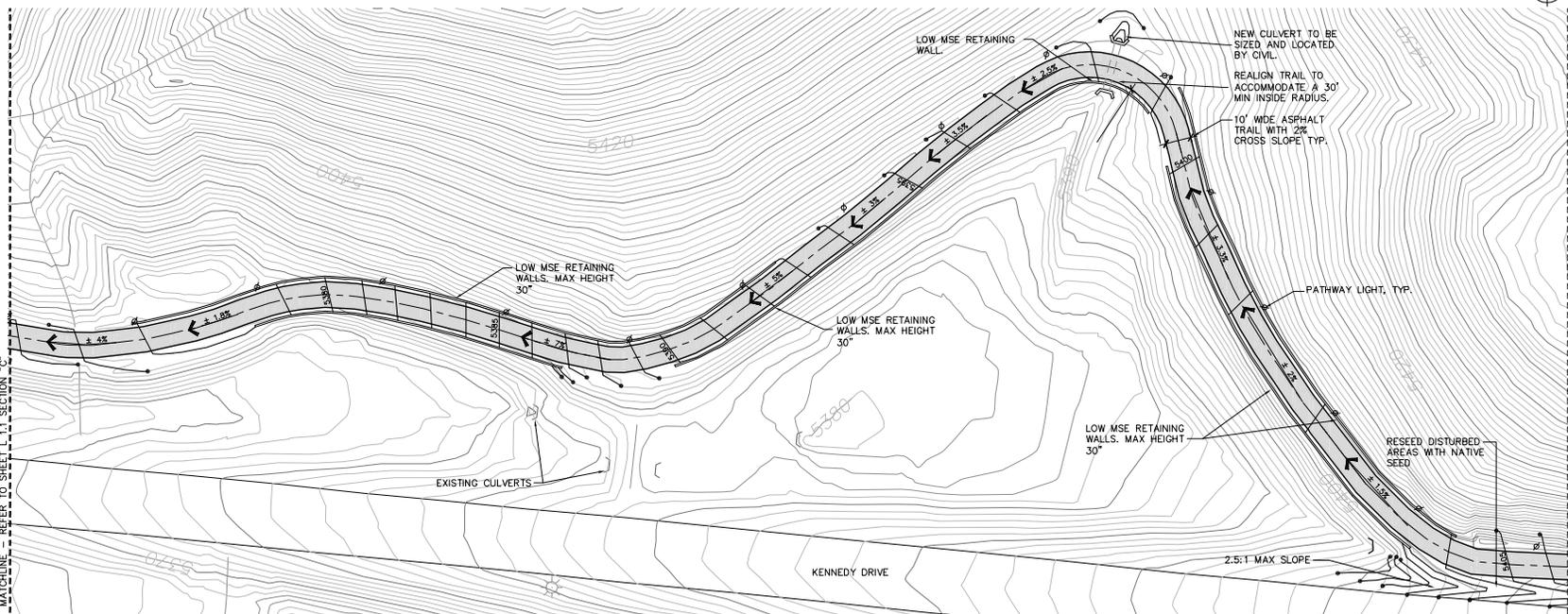
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LAYOUT & GRADING PLAN  
SHEET NUMBER:

# L1.1



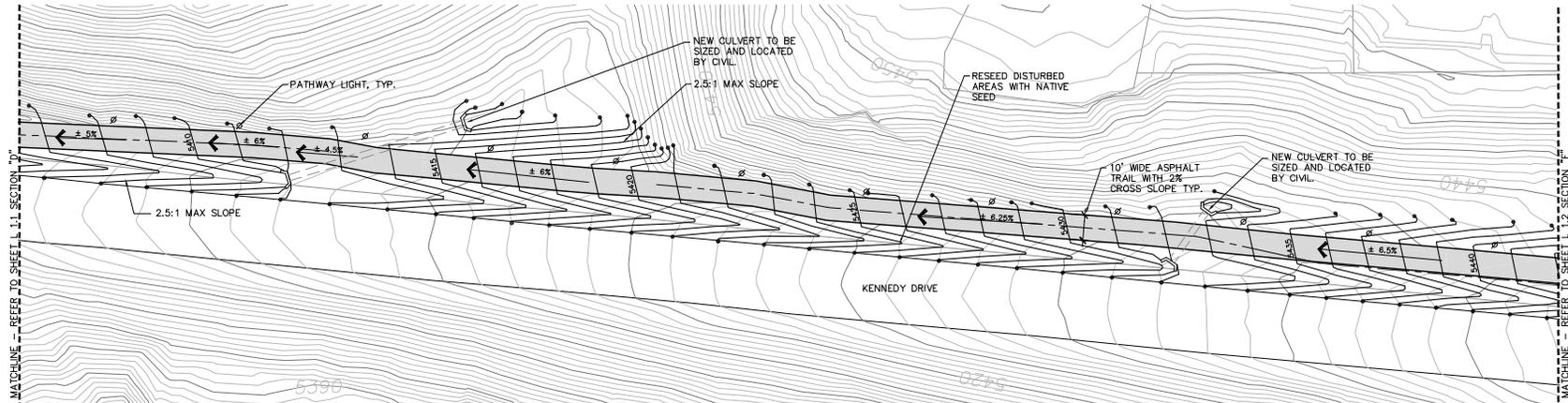
KENNEDY DRIVE TRAIL - TRAIL SECTION "C"

SCALE = 1" = 20'-0"



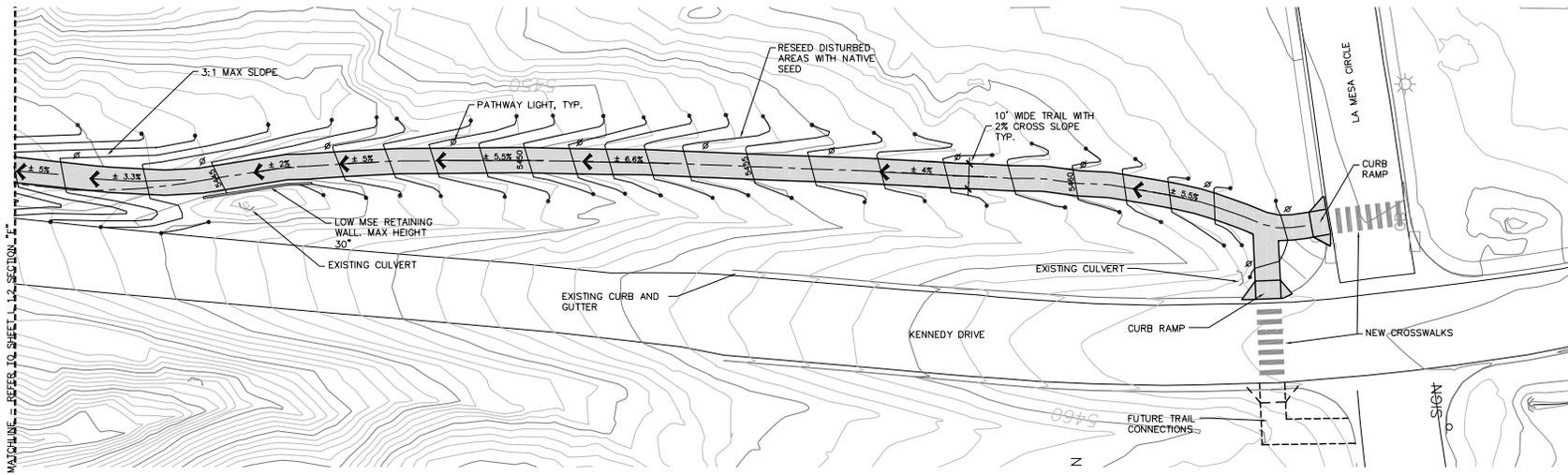
KENNEDY DRIVE TRAIL - TRAIL SECTION "D"

SCALE = 1" = 20'-0"



KENNEDY DRIVE TRAIL - TRAIL SECTION " E "

SCALE = 1" = 20'-0"



KENNEDY DRIVE TRAIL - TRAIL SECTION " F "

SCALE = 1" = 20'-0"

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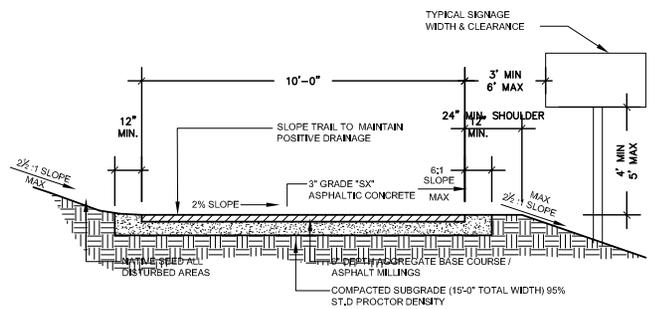
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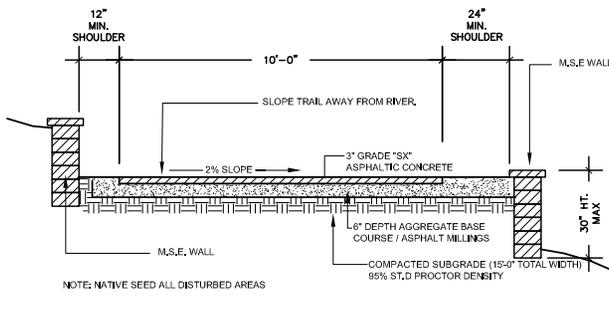
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SHEET TITLE: LAYOUT & GRADING PLAN

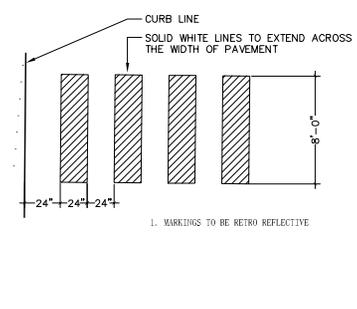
SHEET NUMBER: L1.2



1 10' WIDE BIKEWAY/TRAIL (ASPHALT)

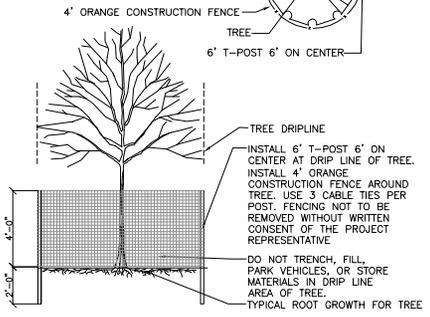


2 TYPICAL BIKEWAY WITH M.S.E. WALL

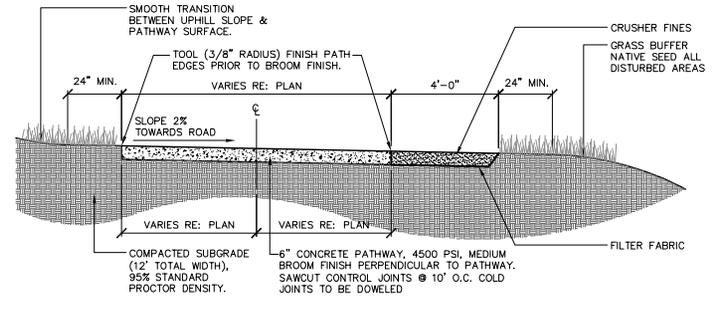


3 PEDESTRIAN CROSSWALK

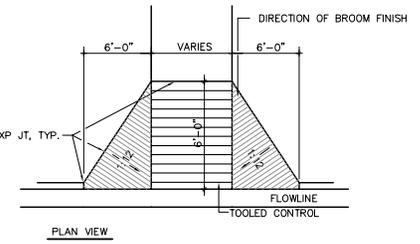
NOTE: REMOVAL OF TREE PROTECTION FENCE DURING CONSTRUCTION WILL RESULT IN A \$500 PER INCIDENT, PER DAY FINE TO THE CONTRACTOR.



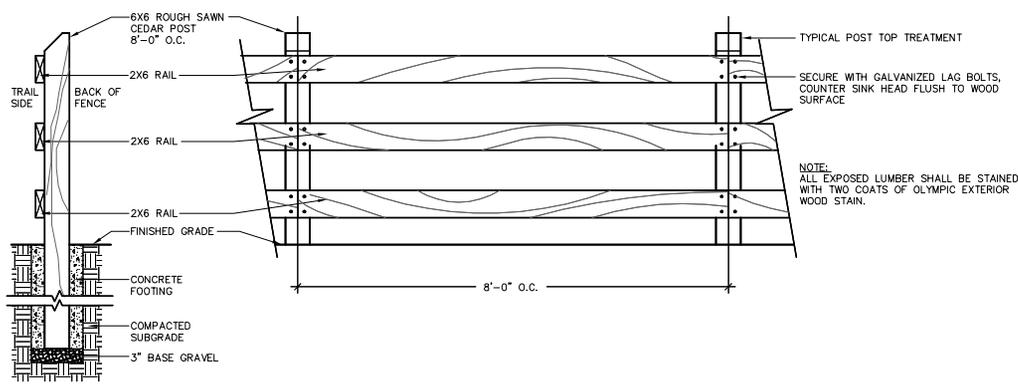
4 TREE PROTECTION



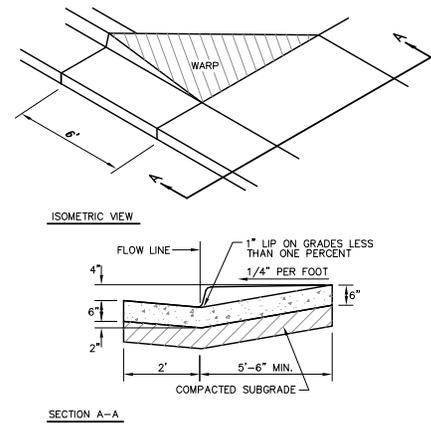
5 CONCRETE / CRUSHER FINE TRAIL



7 ACCESSIBLE CONCRETE RAMP



6 3-RAIL WOOD FENCE



7 ACCESSIBLE CONCRETE RAMP

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Rangely, Colorado

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SITE DETAILS  
SHEET NUMBER:

**SD1**